



# KIAMA LOCAL TRAFFIC COMMITTEE MEETING

To be held at 9.00am on

**Tuesday 5 December 2023**

Council Chambers

11 Manning Street, KIAMA NSW 2533

## **Order of Business**

- 1 Apologies
- 2 Acknowledgement of Traditional owners
- 3 Confirmation of Minutes of Previous Meeting
- 4 Business Arising From The Minutes
- 5 Report of RMS delegated formal items
- 4 General Business
- 5 Closure

## **Members**

Cr Stuart Larkins  
Mr Andy Gaudiosi  
Senior Constable Craig Gray  
Mr Darrell Clingan

30 November 2023

To the Chairman, Councillors and Committee Members:

**NOTICE OF KIAMA LOCAL TRAFFIC COMMITTEE MEETING**

You are respectfully requested to attend an **Kiama Local Traffic Committee Meeting** of the Council of Kiama, to be held in the **Council Chambers**

**11 Manning Street, KIAMA NSW 2533** on **Tuesday 5 December 2023** commencing at **9.00am** for the consideration of the undermentioned business.

Yours faithfully

Michael Malone

**Director Infrastructure and Liveability**

## TABLE OF CONTENTS

ITEM	SUBJECT	PAGE NO
1	<b>APOLOGIES .....</b>	<b>4</b>
2	<b>ACKNOWLEDGEMENT OF TRADITIONAL OWNERS .....</b>	<b>4</b>
3	<b>CONFIRMATION OF MINUTES OF PREVIOUS MEETING .....</b>	<b>5</b>
3.1	Kiama Local Traffic Committee Meeting on 7 November 2023 .....	5
4	<b>BUSINESS ARISING FROM THE MINUTES.....</b>	<b>12</b>
5	<b>REPORT OF RMS DELEGATED FORMAL ITEMS .....</b>	<b>13</b>
5.1	Implementation of changes to timed parking zone - Gerringong Post Office.....	13
5.2	Provision of 'No Parking' zone - 33 Shoalhaven Street, Kiama.....	16
5.3	Proposed road closure - Red Hot Summer Tour - 6 January 2024 .....	19
5.4	Road Closure - Belinda Street, Gerringong NSW 2534 .....	26
5.5	Terralong Street Underbridge - Replacement Collision Protection Beams.....	32
5.6	Proposed Road Closure - South Coast Tour.....	44
4	<b>GENERAL BUSINESS.....</b>	<b>82</b>
5	<b>CLOSURE .....</b>	<b>82</b>

**AGENDA FOR THE  
KIAMA LOCAL TRAFFIC COMMITTEE MEETING OF KIAMA  
MUNICIPAL COUNCIL  
TUESDAY 5 DECEMBER 2023**

**1 APOLOGIES**

**2 ACKNOWLEDGEMENT OF TRADITIONAL OWNERS**

*"I would like to acknowledge the traditional owners of the land on which we meet, the Wadi Wadi people of the Dharawal nation, and pay my respect to Elders past and present."*

### 3 CONFIRMATION OF MINUTES OF PREVIOUS MEETING

#### 3.1 Kiama Local Traffic Committee Meeting on 7 November 2023

##### Attachments

- 1 Minutes - Kiama Local Traffic Committee Meeting - 07 11 23 - PDF

##### Enclosures

Nil

#### **RECOMMENDED**

That the Minutes of the Kiama Local Traffic Committee meeting held on 7 November 2023 be received and accepted.



# **MINUTES OF THE KIAMA LOCAL TRAFFIC COMMITTEE MEETING**

**commencing at 9.08am on**

**TUESDAY 7 NOVEMBER 2023**

Council Chambers  
11 Manning Street, KIAMA NSW 2533

---

**MINUTES OF THE KIAMA LOCAL TRAFFIC COMMITTEE 7 NOVEMBER 2023**

---

**MINUTES OF THE KIAMA LOCAL TRAFFIC COMMITTEE  
HELD IN THE COUNCIL CHAMBERS, KIAMA  
ON TUESDAY 7 NOVEMBER 2023 AT 9.00AM**

**PRESENT:** Stuart Larkins (Councillor Representative), Justin Miles (Transport for NSW Representative), Darrell Clingan (Local Member's Representative), Michaela Sisk (Manager Engineering and Technical) (Chairperson), Chris Balzarano (Council Ranger), Kim Duffy (Kiama Coaches), and Alyson Hodgekiss (Minutes)

**1 APOLOGIES**

Michael Malone (Director Infrastructure and Liveability), Craig Gray (NSW Police Representative), & Janelle Burns (Kiama Council Road Safety Officer)

**2 ACKNOWLEDGEMENT OF TRADITIONAL OWNERS**

The Chair declared the meeting open and acknowledged the traditional owners:

*"On behalf of those present, I would like to show my respect and acknowledge the traditional owners of the Land, of Elders past and present, on which this meeting takes place, and extend that respect to other Aboriginal and Torres Strait Islander people present."*

**3 CONFIRMATION OF MINUTES OF PREVIOUS MEETING**

**3.1 Kiama Local Traffic Committee Meeting Minutes on 3 October 2023**

[23/039LTC](#)

**Resolved** that the Minutes of the Kiama Local Traffic Committee meeting held on 3 October 2023 be received and accepted.

(Member Stuart Larkins and Member Justin Miles)

**4 BUSINESS ARISING FROM THE MINUTES**

Nil

**5 REPORT OF RMS DELEGATED FORMAL ITEMS**

**5.1 Proposed road closure - Kiama Triathlon - Minnamurra**

[23/040LTC](#)

---

**MINUTES OF THE KIAMA LOCAL TRAFFIC COMMITTEE    7 NOVEMBER 2023**

---

**Recommended that**

the Kiama Local Traffic Committee defers the matter until:

1. Parking is to be limited along the route.
2. A copy of the letter that will be provided via letter drop to residents advising of the impacts of the event is to be provided to the Traffic Committee. It is expected that residents are not going to react well to the restrictions and impacts they are facing from this event.
3. Police are engaged or consulted with for this event, to facilitate the safety of the event. Section 115 of the Road Transport Act relates to racing on public roads, requiring written approval from NSW Police before the events may proceed. Early engagement with NSW Police prior is recommended given the complexity of the area. Further information roles of approving agencies is captured in New South Wales Guidelines for Bicycle Road Races Version 2.0. A link to the guideline is can be found on TfNSW web page [Event management guidelines | Transport for NSW](#).
4. The plan showing emergency access for residents who need to leave or enter their home in the event of an emergency is clarified and detailed in the Traffic Management Plan.
5. Hostile mitigation measures are to be provided at areas where there a concentration of vehicles and cyclists e.g. the roundabout intersection at Riverside Drive and Oxley Ave. NSW Police can provide advice of more areas. These are to be shown on the Traffic Management Plan.
6. Additional detail is given around the underpass with the stop sign to ensure cyclists aren't breaking road rules. Refer to above guidelines for details and guidance.
7. The M1 off ramp at Riverside Drive (Minnamurra interchange) is proposed to be closed for this event. This will require a Road Occupancy Licence and endorsement from the TfNSW Regional Events Team. A speed zone reduction on the off ramp is also recommended to facilitate pedestrian crossing movement.
8. Some of the road closures will cause issues as there are no detours available, including Swamp Road and Riverside Drive. This will cause issues for bus services, waste services, milk trucks, and quite a lot of residents needing to access their properties
9. More research is conducted as to the most appropriate day for this event, to cause the least amount of disruption with residents, bus runs, garbage pickups, and also communication needs to be undertaken with the golf club, to see what the expected numbers are for golfers for Saturday and Sunday.
10. More research needs to be conducted as to how to close down the water areas proposed for the event, that the public use for swimming, boats, paddleboards, etc.

(Member Stuart Larkins and Member Justin Miles)



---

MINUTES OF THE KIAMA LOCAL TRAFFIC COMMITTEE 7 NOVEMBER 2023

---

**5.2 Kiama Sky Show - New Years Eve - Road Closure**

**23/041LTC**

**Resolved** that the Committee endorses the traffic changes associated with the Kiama Sky Show event at Black Beach Reserve, to be held on Sunday 31 December 2023, to implement proposed traffic changes, subject to the organisers and suitably qualified traffic controllers complying with the following conditions:

1. All road closures, diversions and traffic changes be undertaken by suitably qualified traffic controllers in compliance with the Traffic Control Plan TLTGS-6618-41549 prepared by Traffic Logistics Pty Ltd.
2. The organisers be responsible for the supply, erection and removal of all traffic control devices, equipment and personnel associated with the implementation of the approved Traffic Control Plan. Please note that the VMS Board shown on the corner of Collins and Terralong Street (Hindmarsh Park side) on page 145 of the Traffic Management Plan will need to be placed elsewhere, as it is an offence to place a VMS sign on a Council nature strip.
3. The following provisions be made to address anti-terrorism, hostile vehicle guidelines and provide a safe environment for attendees to the event:
  - a. Road closure on a section of Terralong Street, from Railway Parade to Manning Street roundabout, between 2pm and 10:30pm and from Railway Parade to Collins Street roundabout, between 4pm and 10:30pm; intended to allow for attendees to exit the event using Terralong Street as a pedestrian zone free from vehicles until 10:30pm.
  - b. Road closure on a section of Shoalhaven Street, from Minnamurra Street roundabout to Terralong Street, from 4pm – 10:30pm; intended to provide a safe environment for attendees to walk between the event sites in Hindmarsh Park & Black Beach reserve; and amplify the event by delivering an 'Eat Street' on Shoalhaven Street.
  - c. Road closure on a section of Stobo Road, between 8pm to 9:30pm for the fireworks exclusion zone.
  - d. Road closure of Bong Bong Street on either side of the Bong Bong Street / Manning Street roundabout, 8pm to 10:30pm, to guide traffic from the Blowhole.
4. Lodgement with Council of an application for a Section 138 Roads Act permit and payment of associated fee.
5. Notification of proposed traffic changes be given to police, local emergency services, businesses and residents effected by the closure.
6. An advertisement be placed in the local media advising of the closure.

(Member Stuart Larkins and Member Justin Miles)

---

**MINUTES OF THE KIAMA LOCAL TRAFFIC COMMITTEE 7 NOVEMBER 2023**

---

**5.3 Kiama Show - 2024 - Road Closures**

**23/042LTC**

**Resolved** that the Committee endorsed that the traffic changes associated with the event on Friday 26 January 2024 and Saturday 27 January 2024 from 5 a.m. to 10 p.m. each day be approved, subject to organisers complying with the following conditions:

1. The event organiser complies with the Traffic Management Plan shown in Attachment 1.
2. Notification of the closures be given to Police, local emergency services, businesses and affected residents.
3. An advertisement be placed in the local media advising of the closure a minimum of seven days prior to the event.

(Member Stuart Larkins and Member Justin Miles)

**5.4 Crooked River Winery - Dave Ferrit Festival - Traffic Mangement Plan**

*This report has been withdrawn as event was postponed to June 2024.*

**6 GENERAL BUSINESS**

**6.1 Cars parking in bus zone on Wednesday afternoons on Manning Street in front of SS Peter and Paul Catholic School**

KD from Kiama Coaches advised that there has been significant issues with people parking in the bus zone in front of Ss Peter and Paul Catholic School on Wednesday afternoons whilst the Farmers Markets are on. The buses have been able to pull into the bus zone and safely pick up children. CB advised that he has been aware of this and has been regularly patrolling the zone each Wednesday, and has issued over 50 infringements.

**6.2 Complimentary breath testing by TfNSW at the Changing Tides Event**

JM advised that TfNSW will be conducting complimentary breath testing and Plan B behavioural messaging at the upcoming Changing Tides Event in Kiama on 16 and 17 December 2023, to support people attending that event to make good decisions in getting home.

---

**MINUTES OF THE KIAMA LOCAL TRAFFIC COMMITTEE 7 NOVEMBER 2023**

---

**6.3 Linemarking required on Fern Street, Gerringong for resurfaced area.**

SL advised that the section of road on Fern Street, Gerringong that was resurfaced roughly two months ago urgently needs the linemarking to be redone.

**7 CLOSURE**

There being no further business the meeting closed at 10.00am

These Minutes were confirmed at the Ordinary Meeting of Council held on 5 December 2023

.....  
Director Engineering & Works

**4 BUSINESS ARISING FROM THE MINUTES**

## 5 REPORT OF RMS DELEGATED FORMAL ITEMS

### 5.1 Implementation of changes to timed parking zone - Gerringong Post Office

CSP Objective: Outcome 4.3: Our built environment is safe and inclusive; our infrastructure is well planned and well managed.

CSP Strategy: 4.3.2 Actively plan how we travel and create safe travel routes whether we are walking, cycling, driving or accessing public transport.

Delivery Program: 4.3.2.2 Implement road safety and traffic management programs and responsibilities

Item 5.1

#### Summary

Consideration for the implementation of changes to the existing timed 'No Parking' zone adjacent the Gerringong Post Office Fern Street Gerringong, from 5pm 6pm only to 8:00am-9:00am MON – FRI and 4:30pm-5:30pm MON - FRI.

#### Financial implication

Nil

#### Risk implication

Nil

#### Policy

Nil

#### Consultation (internal)

Director Infrastructure & Liveability  
Manager Engineering and Technical Services  
Road Safety Officer

#### Communication/Community engagement

N/A

#### Attachments

- 1 Gerringong Post office R5-40 no parking

#### Enclosures

Nil

### **RECOMMENDATION**

That the Committee endorses the changes to the existing timed 'Mail Zone' at Sop 3 146-152 Fern Street, Gerringong, the Gerringong Post office, identifying **8:00am-9:00am and 4:30pm-5:30pm Monday to Friday as a no parking zone**, through the installation of regulatory signage and line marking, as shown in the attached plan.

## Report of RMS delegated formal items

## 5.1 Implementation of changes to timed parking zone - Gerringong Post Office (cont)

**Background**

Council has been advised of parking issues around the Gerringong Post Office. At present the 'Mail Zone' street sign is from 5pm-6pm from Monday to Friday only which does not assist the Australia Post trucks arriving in the morning and afternoon, as the current zoned parking times do not match the current two time slots of deliveries and pickups from the Australia Post. Morning drops by Australia Post is between 8am and 9am and the afternoon trucks arrive around 4:45pm for the 5pm pick-up.

Further, there are issues around vehicles ignoring the 'no parking' signs with vehicles parking in the mail zone causing Australia Post drivers having to park right up to and sometimes past the restricted signposts making loading and unloading of the trucks difficult.

The Manager of the Post Office is requesting changes to the no parking hours to support the current mail delivery and pick-up by Australia Post.

On this basis, it is proposed to introduce changes to timed parking "Mail Zone" from 5pm 6pm only to 8:00am-9:00am Monday through Friday and 4:30pm-5:30pm Mon through Fri. Regulatory signage and line marking at this location to be installed as per the attached signage and line marking plan with the aim to provide the following road safety benefits:

- Improve short term parking including pick up and drop offs.
- Improve loading and unloading for Post Office users.
- Improve parking compliance with the NSW road rules.
- Improve parking turnover for Post Office users.



Existing conditions – Gerringong Post Office

DO NOT SCALE

SIGNAGE SCHEDULE	
CODE	OFF
R5-40 (L)	1
R5-40 (L)	1

**S1**

NO PARKING FOR 2 HOURS  
MON-FRI 9AM-5PM

**S2**

NO PARKING FOR 2 HOURS  
MON-FRI 9AM-5PM

PROJECT NO.	2023-23	DATE	2023-23	DRAWN BY	A
TITLE	KIAMA MUNICIPAL COUNCIL R5-40 NO PARKING Gerringong Post Office				
PROJECT NO.	C01				
DATE	1/1/2023				
DRAWN BY	A				

**5.2 Provision of 'No Parking' zone - 33 Shoalhaven Street, Kiama**

CSP Objective: Outcome 4.3: Our built environment is safe and inclusive; our infrastructure is well planned and well managed.

CSP Strategy: 4.3.2 Actively plan how we travel and create safe travel routes whether we are walking, cycling, driving or accessing public transport.

Delivery Program: 4.3.2.2 Implement road safety and traffic management programs and responsibilities

Item 5.2

**Summary**

Consideration of the implementation of a 'No Parking' zone opposite 33 Shoalhaven Street, Kiama between 6am and 9am Mondays with the installation of regulatory signage and linemarking.

**Financial implication**

Nil

**Risk implication**

Nil

**Policy**

Nil

**Consultation (internal)**

Director Infrastructure & Liveability

Road Safety Officer

**Communication/Community engagement**

Nil

**Attachments**

1 No Parking - Shoalhaven Street Kiama

**Enclosures**

Nil

**RECOMMENDATION**

That the Committee endorses the implementation of a 'No Parking' at 33 Shoalhaven Street, Kiama, through the installation of regulatory signage, as shown in the attached plan.



Report of RMS delegated formal items

5.2 Provision of 'No Parking' zone - 33 Shoalhaven Street, Kiama (cont)

---

**Background**

Concerns have been raised from Council's Waste Services in relation to the ability for service/delivery trucks and waste collection trucks to safely carry out garbage collection services at the front of 33 Shoalhaven Street, Kiama due to vehicles parking in this location.

It is proposed that a 'No Parking' zone with regulatory signage be installed at this location, as per the attached signage and line marking plan, as reported by Council Waste Services staff who are having continuing issues with parked cars and servicing bins at 33 Shoalhaven Street, Kiama. Waste Services have requested a 'No Parking' zone to be installed between 6am and 9am on Mondays only.

**Item 5.2**



**5.3 Proposed road closure - Red Hot Summer Tour - 6 January 2024**

CSP Objective: Outcome 1.1: We want a strong sense of community and belonging, where social and cultural life can flourish; and our families, friends and visitors feel welcome and included.

CSP Strategy: 1.1.1 Provide spaces, services and initiatives that foster a proud, inclusive, and connected community for all.

Delivery Program: 1.1.1.2 Partner with the community, other levels of government and key stakeholders to provide support to community and cultural engagement sectors, community groups and organisations.

Item 5.3

**Summary**

The Red Hot Summer Tour will be held at Kiama Showground on the 14 March 2020. The event organizer proposes closing off Bong Bong Street from Manning Street to the entrance of the Showground from 6 a.m. to 11 p.m. to assist in crowd control.

**Financial implication**

Nil

**Risk implication**

Traffic/parking congestion and pedestrians walking within the vehicle travel lanes to the site creating high risk road safety issues. A Traffic Management Plan has been prepared for the event to address both vehicular and pedestrian access and safety.

**Policy**

Development consent compliance under the EP&A Act.

Section 138 Road Occupancy under the Roads Act.

**Consultation (internal)**

Consultation with Manager Engineering & Technical Services and Manager Tourism & Events

**Communication/Community engagement**

Kiama Local Traffic Committee

**Attachments**

1 Traffic Management Plan - Red Hot Summer Tour 2024

**Enclosures**

Nil

**RECOMMENDATION**

Report of RMS delegated formal items

5.3 Proposed road closure - Red Hot Summer Tour - 6 January 2024 (cont)

---

### **RECOMMENDATION**

That the Committee endorses the traffic changes associated with the event on 6 January 2024 from 6:00 a.m. to 11.00 p.m. be approved subject to organizers complying with the following conditions:

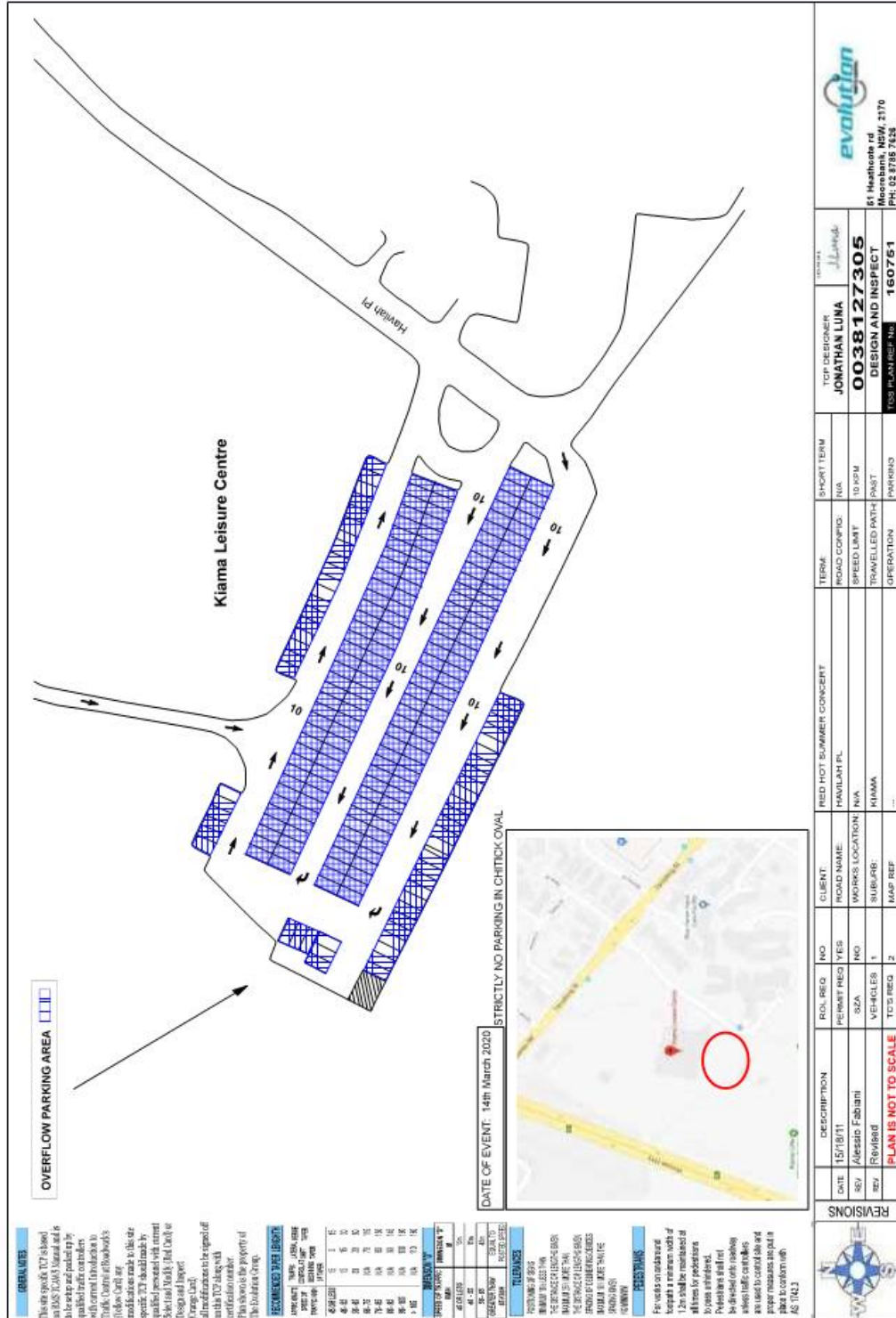
1. The event organizer complies with the Traffic Management Plans shown in Attachment 1.
2. The organizers shall be responsible for the supply, erection and removal of all traffic control devices, equipment and personnel associated with the implementation of the approved Traffic Management Plan.
3. Lodgment with Council of an application for a Section 138 Roads Act permit and payment of associated fee.
4. Notification of proposed traffic changes be given to police, local emergency services, businesses and residents affected by the closure.
5. An advertisement be placed in the local media advising of the closure and traffic changes.
6. Proof of public liability insurance complying with the Australian Prudential Regulatory Authority (APRA) being provided to Council prior to the event.

### **Background**

The Red Hot Summer Tour is a music event featuring contemporary performers and bands and is expected to attract up to 6000 patrons. The event will be at Kiama Showground on Saturday 6 January 2024.

The Regional Touring Company Pty Ltd proposes to close off Bong Bong Street from Manning Street to the entrance of the Showground from 6 a.m. to 11 p.m. to assist in crowd control. The road closure plan is similar what has been implemented in previous years and is included in Attachment 1 of this report.

Traffic Management Plan







## RED HOT SUMMER CONCERT - KIAMA

**CLIENT DETAILS**  
 Client Name: **Fresh Is Fresh Pty Ltd Deb**  
 Client Contact Name: **ALEXANDER**  
 Client Contact Number: **0414 867 738**  
 PO/Contract Number: **N/A**

**PROJECT/SITE OVERVIEW**  
 Project/Site Description: **RED HOT SUMMER CONCERT**  
 Location: **KIAMA**  
 Anticipated Commencement Date: **14/03/2020**  
 Estimated Duration of Works: **Working Hours**

**SCOPE OF WORKS**  
 This Traffic Guidance Scheme(s) has been developed to allow the client to conduct works at the above location and to display a commitment to Traffic and Pedestrian Management, Reporting, and Reviewing.  
 These works will include, but not limited to:  
**EVENT - TRAFFIC CONTROL TO ASSIST WITH ROAD CLOSURE**

**PEDESTRIAN & CYCLIST MANAGEMENT**  
 As required all pedestrian traffic and cyclists, for the duration of the construction works, will be monitored as required. Appropriate pedestrian warning signage and directional signage will be in place and monitored throughout the upgrade works as per the provided TGS's attached to this document. Where trip hazards exist or are identified 'Pedestrian Watch Your Step' T8-1 SHALL be installed where appropriate space allows. Pedestrians may be directed around footpath works via detours and/or side tracks.

**GENERAL NOTES**  
 - Preparing this TGS has ensured to comply with the AS 1742.3-2009 (Fourth Issue, Apr 2009). Any unapproved variations to the design will negate their liability. Variations and amendments to this TGS are to be recorded on this TGS with the changes noted, along with the date and initials of the change and the accreditation details of the person making the change.  
 - The attached TGS's SHALL be read in conjunction with the notes page and the associated risk assessments and an on site risk assessment SHALL be performed before any implementation works takes place.  
 - It is the Client's responsibility to ensure they have a copy of the permits (in date) for the closure being implemented.  
 - This TGS SHALL only be implemented by a competent person(s) with a current Traffic Management Implementation (TMI) qualification.  
 - A toolbox talk is to take place before works commencing.  
 - Work Site Safety Traffic Management Checklist to be filled out prior to implementation, and upon completion.  
 - Traffic Controllers to identify and make note of escape routes prior to commencement of works.  
 - Hand held traffic lights are to be utilised where required to communicate with road users and other vehicles.  
 - Principal Contractor to notify local Emergency Services in advance of commencing works.

**SITE SPECIFIC NOTES** Ⓞ Where this symbol appears, please refer back to the corresponding number located on the notes page

- Advance signs SHALL be displayed as prominently as possible by selecting the longitudinal location of the sign for best sight distance for approaching traffic. Signs continuously required for works which will be in progress for periods longer than 2 weeks should be erected in a permanent manner, e.g. on private property, and duplicated.  
 2. Worksite symbols SHALL be removed or covered when workers are no longer visible to traffic. (AS/MUTCD 3.4.4)  
 3. Speed of the traffic SHALL be reduced to 40km/h when workers on foot will be within 1.2m of traffic. (AS/MUTCD 4.2.c.ii)  
 4. Traffic Controller Ahead/Prepare to Stop sign SHALL be used when a traffic controller is attending traffic. It SHALL be installed at the greater value of 2D in advance of the traffic control position.  
 The sign SHALL NOT be displayed when the traffic controller is not in attendance controlling traffic. (AS/MUTCD 3.5.2.a8b)  
 5. Existing or conflicting signage SHALL be covered or removed when this TGS is operational. (AS/MUTCD 2.4.4)  
 6. Access to local businesses and driveways will be maintained during works. Unless otherwise shown on the TGS(s) and site specific notes, blocking public and private access is not permitted prior to commencement of works.  
 7. Access to bus stops to be maintained where possible. If not possible, the client is to consult and gain written approval from Transport NSW will be required as per TMC requirements (PH: 131 500).  
 8. Cones & Bollards SHALL be 700mm or taller. Taper Cone Spacings and all other Cone Spacings as per Table 3.7 Recommended Max Spacing of Cones & Bollards.  
 9. Traffic volumes should be monitored throughout the implementation of the TGS(s). (MUTCD 3.9.1)  
 10. Where practicable, signs SHALL be erected on both sides of the roadway on multilane divided or one-way roads where the volume is 10000 vpd or greater. (AS/MUTCD 2.5.2)  
 11. An active traffic control position, under conditions of heavy traffic or high speeds, is required for the duration of the works. Additional advance warning may be required to avoid end-of-queue collisions. (AS/MUTCD 4.7.8)  
 12. Minimum lane widths SHALL be in accordance with clause 4.13.3. Where delineation is present, a clearance area between the edge of traffic lane and delineation SHALL be provided. Measurements for this clearance are outlined under MUTCD 4.13.4.

**IMPLEMENTATION & RECOVERY OF DEVICES**  
 Before work commences, signs and devices at the approaches to and within the work area SHALL be implemented in accordance with the approved Traffic Guidance Schemes and the Traffic Control Companies Safe Work Method Statements, in the following sequence:  
 (a) Advance warning signage.  
 (b) All intermediate advance warning and regulatory signs and devices required in advance of the taper or start of the work area.  
 (c) All delineating devices required to form the taper including the illuminated flashing arrow sign at the end of the taper where required.  
 (d) Delineation of the work area or side track.  
 (e) All other required warning and regulatory signs including termination and end of temporary speed zone signs.  
 Signs and devices should be positioned and erected so that:  
 (i) they are properly displayed (see Clause 3.3);  
 (ii) they do not obscure the sight of the relevant road user;  
 (c) they cannot be obscured from view (e.g. by vegetation or parked cars);  
 (d) they do not obscure other devices from the line of sight of the intended road user;  
 (e) they do not become a possible hazard to workers, pedestrians or vehicles; and  
 (f) they do not deflect traffic into an undesirable path.  
 (g) they do not restrict sight distance for drivers entering from side roads or streets, or private driveways; and  
 (h) they are not installed using supports that could be a hazard if struck by a vehicle.  
 Delineating devices (e.g. traffic cones, bollards, post mounted delineators) should generally be placed 1 m clear of the travelled path where practicable. The taper path or to scale opening traffic, used to define the edge of the travelled path or to scale opening traffic, length of tapers or markings:  
 (i) Minimum, 10% less than the distances or lengths given.  
 (ii) Maximum, 25% more than the distances or lengths given.  
 Tolerances on Spacing of Delineating Devices:  
 (i) Maximum, 10% more than the spacing shown; (ii) No minimum.  
 Recovery of devices at the conclusion of the work shall be done in the reverse order using the same work method as for setting out.

**RECORDING & MONITORING**  
 Details of all changes in traffic movements shall be recorded and maintained throughout the construction period and submitted within 7 days from the date of practical completion. Regular inspections of traffic control devices SHALL be carried out a minimum of three daily and recorded in the Daily Traffic Management Report. The Client shall be notified of any changes to the TGS's which will be held on file with the Client Work Forum. In the event of a traffic related incident with in the Site, The Client SHALL immediately notify the principal's representative, the police, and any necessary emergency services.

**CLIENT DETAILS**  
 Client Name: **Fresh Is Fresh Pty Ltd Deb**  
 Client Contact Name: **ALEXANDER**  
 Client Contact Number: **0414 867 738**  
 PO/Contract Number: **N/A**

**PROJECT/SITE OVERVIEW**  
 Project/Site Description: **RED HOT SUMMER CONCERT**  
 Location: **KIAMA**  
 Anticipated Commencement Date: **14/03/2020**  
 Estimated Duration of Works: **Working Hours**

**SCOPE OF WORKS**  
 This Traffic Guidance Scheme(s) has been developed to allow the client to conduct works at the above location and to display a commitment to Traffic and Pedestrian Management, Reporting, and Reviewing.  
 These works will include, but not limited to:  
**EVENT - TRAFFIC CONTROL TO ASSIST WITH ROAD CLOSURE**

**PEDESTRIAN & CYCLIST MANAGEMENT**  
 As required all pedestrian traffic and cyclists, for the duration of the construction works, will be monitored as required. Appropriate pedestrian warning signage and directional signage will be in place and monitored throughout the upgrade works as per the provided TGS's attached to this document. Where trip hazards exist or are identified 'Pedestrian Watch Your Step' T8-1 SHALL be installed where appropriate space allows. Pedestrians may be directed around footpath works via detours and/or side tracks.

**GENERAL NOTES**  
 - Preparing this TGS has ensured to comply with the AS 1742.3-2009 (Fourth Issue, Apr 2009). Any unapproved variations to the design will negate their liability. Variations and amendments to this TGS are to be recorded on this TGS with the changes noted, along with the date and initials of the change and the accreditation details of the person making the change.  
 - The attached TGS's SHALL be read in conjunction with the notes page and the associated risk assessments and an on site risk assessment SHALL be performed before any implementation works takes place.  
 - It is the Client's responsibility to ensure they have a copy of the permits (in date) for the closure being implemented.  
 - This TGS SHALL only be implemented by a competent person(s) with a current Traffic Management Implementation (TMI) qualification.  
 - A toolbox talk is to take place before works commencing.  
 - Work Site Safety Traffic Management Checklist to be filled out prior to implementation, and upon completion.  
 - Traffic Controllers to identify and make note of escape routes prior to commencement of works.  
 - Hand held traffic lights are to be utilised where required to communicate with road users and other vehicles.  
 - Principal Contractor to notify local Emergency Services in advance of commencing works.

**SITE SPECIFIC NOTES** Ⓞ Where this symbol appears, please refer back to the corresponding number located on the notes page

- Advance signs SHALL be displayed as prominently as possible by selecting the longitudinal location of the sign for best sight distance for approaching traffic. Signs continuously required for works which will be in progress for periods longer than 2 weeks should be erected in a permanent manner, e.g. on private property, and duplicated.  
 2. Worksite symbols SHALL be removed or covered when workers are no longer visible to traffic. (AS/MUTCD 3.4.4)  
 3. Speed of the traffic SHALL be reduced to 40km/h when workers on foot will be within 1.2m of traffic. (AS/MUTCD 4.2.c.ii)  
 4. Traffic Controller Ahead/Prepare to Stop sign SHALL be used when a traffic controller is attending traffic. It SHALL be installed at the greater value of 2D in advance of the traffic control position.  
 The sign SHALL NOT be displayed when the traffic controller is not in attendance controlling traffic. (AS/MUTCD 3.5.2.a8b)  
 5. Existing or conflicting signage SHALL be covered or removed when this TGS is operational. (AS/MUTCD 2.4.4)  
 6. Access to local businesses and driveways will be maintained during works. Unless otherwise shown on the TGS(s) and site specific notes, blocking public and private access is not permitted prior to commencement of works.  
 7. Access to bus stops to be maintained where possible. If not possible, the client is to consult and gain written approval from Transport NSW will be required as per TMC requirements (PH: 131 500).  
 8. Cones & Bollards SHALL be 700mm or taller. Taper Cone Spacings and all other Cone Spacings as per Table 3.7 Recommended Max Spacing of Cones & Bollards.  
 9. Traffic volumes should be monitored throughout the implementation of the TGS(s). (MUTCD 3.9.1)  
 10. Where practicable, signs SHALL be erected on both sides of the roadway on multilane divided or one-way roads where the volume is 10000 vpd or greater. (AS/MUTCD 2.5.2)  
 11. An active traffic control position, under conditions of heavy traffic or high speeds, is required for the duration of the works. Additional advance warning may be required to avoid end-of-queue collisions. (AS/MUTCD 4.7.8)  
 12. Minimum lane widths SHALL be in accordance with clause 4.13.3. Where delineation is present, a clearance area between the edge of traffic lane and delineation SHALL be provided. Measurements for this clearance are outlined under MUTCD 4.13.4.

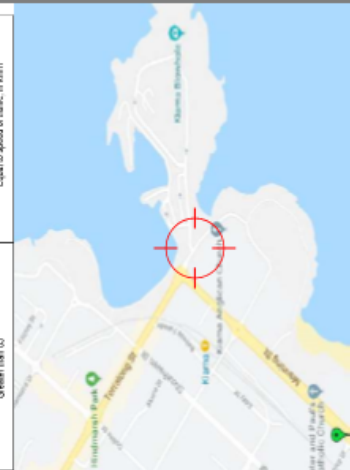
**IMPLEMENTATION & RECOVERY OF DEVICES**  
 Before work commences, signs and devices at the approaches to and within the work area SHALL be implemented in accordance with the approved Traffic Guidance Schemes and the Traffic Control Companies Safe Work Method Statements, in the following sequence:  
 (a) Advance warning signage.  
 (b) All intermediate advance warning and regulatory signs and devices required in advance of the taper or start of the work area.  
 (c) All delineating devices required to form the taper including the illuminated flashing arrow sign at the end of the taper where required.  
 (d) Delineation of the work area or side track.  
 (e) All other required warning and regulatory signs including termination and end of temporary speed zone signs.  
 Signs and devices should be positioned and erected so that:  
 (i) they are properly displayed (see Clause 3.3);  
 (ii) they do not obscure the sight of the relevant road user;  
 (c) they cannot be obscured from view (e.g. by vegetation or parked cars);  
 (d) they do not obscure other devices from the line of sight of the intended road user;  
 (e) they do not become a possible hazard to workers, pedestrians or vehicles; and  
 (f) they do not deflect traffic into an undesirable path.  
 (g) they do not restrict sight distance for drivers entering from side roads or streets, or private driveways; and  
 (h) they are not installed using supports that could be a hazard if struck by a vehicle.  
 Delineating devices (e.g. traffic cones, bollards, post mounted delineators) should generally be placed 1 m clear of the travelled path where practicable. The taper path or to scale opening traffic, used to define the edge of the travelled path or to scale opening traffic, length of tapers or markings:  
 (i) Minimum, 10% less than the distances or lengths given.  
 (ii) Maximum, 25% more than the distances or lengths given.  
 Tolerances on Spacing of Delineating Devices:  
 (i) Maximum, 10% more than the spacing shown; (ii) No minimum.  
 Recovery of devices at the conclusion of the work shall be done in the reverse order using the same work method as for setting out.

**RECORDING & MONITORING**  
 Details of all changes in traffic movements shall be recorded and maintained throughout the construction period and submitted within 7 days from the date of practical completion. Regular inspections of traffic control devices SHALL be carried out a minimum of three daily and recorded in the Daily Traffic Management Report. The Client shall be notified of any changes to the TGS's which will be held on file with the Client Work Forum. In the event of a traffic related incident with in the Site, The Client SHALL immediately notify the principal's representative, the police, and any necessary emergency services.

RECOMMENDED MAXIMUM SPACING OF CONES	
Purpose and usage	Recommended maximum spacing, m (see Clause 4.4.16)
All purposes	≤ 5.0
Cones in an approach to a traffic controller	4
Cones in a position	All cases (see Clause 4.6.4)
Outer edge of traffic lane – e.g. works on shoulder or parking lane	51-70
Separating opposing traffic on 2-lane, 2-way road – e.g. partial or complete lane closure	>70
Separating opposing traffic on a multilane unsealed road – e.g. part of a lane closure	51-70
Adjacent to a closed lane on a multilane unsealed road	51-70
Merge tapers (see Clause 4.8.2)	51-70
Lateral shift tapers (see Clause 4.8.2)	51-70
Protecting freshly painted lines	51-70
† This spacing may be extended to 60m where the length of the line of cones bolards exceeds 15m but not adjacent to bollards where there are workers on foot.	>70
† This spacing may need to be reduced on curves or crests, or if the row of cones is not clearly defined at night.	>70

TABLE 6.2 RECOMMENDED TAPER LENGTHS			
Recommended taper length, m			
Approach speed, km/h	Traffic control at end of taper	Lateral shift taper	Merge taper
45 or less	15	0	15
46-55	15	15	30
56-65	30	30	60
66-75	N/A	70	115
76-85	N/A	80	130
86-95	N/A	90	145
96-105	N/A	100	160
Greater than 105	N/A	110	180

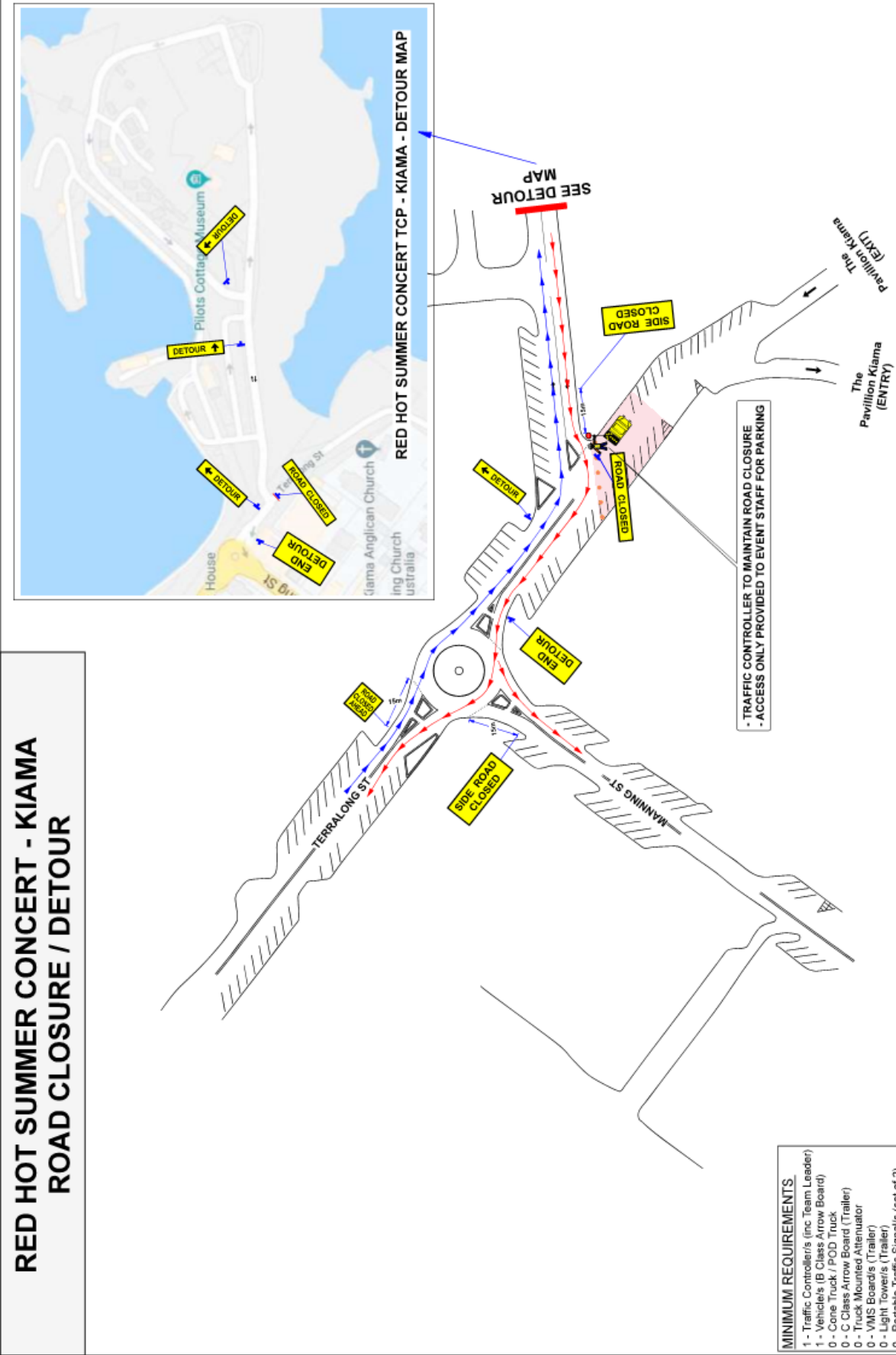
TABLE 4.2 VALUE OF DIMENSION D	
Speed of traffic, km/h	Dimension D, m
≤ 45 or less	0.5
46 to 55	15
56 to 65	45
Greater than 65	Equal to speed of traffic in km/h



ISSUED BY: **Jonathan Luna**  
 AUTHORITY: **evolution**  
 PROJECT NUMBER: **440312981**  
 TGS REF ID: **194202** REV: **01** OF **2**  
 11 HEATHCOTE RD, MOOREBANK NSW 1510 1160 060 041  
 RFB REGISTRATION: CAIRO3070



OPS-FORM-026 TRAFFIC GUIDANCE SCHEME (NSW) | BEST PRINTED IN A3 SIZE



**RED HOT SUMMER CONCERT - KIAMA  
ROAD CLOSURE / DETOUR**

- MINIMUM REQUIREMENTS**
- 1 - Traffic Controller's (inc Team Leader)
  - 1 - Vehicle's (B Class Arrow Board)
  - 0 - Cone Truck / POD Truck
  - 0 - C Class Arrow Board (Trailer)
  - 0 - Truck Mounted Attenuator
  - 0 - VMS Boards (Trailer)
  - 0 - Light Towers (Trailer)
  - 0 - Portable Traffic Signals (set of 2)

**THIS (TGS) SHALL BE READ IN CONJUNCTION WITH NOTES 01**  
IT HAS BEEN DEVELOPED TO ALLOW THE CLIENT TO CONDUCT WORKS AT THE LISTED LOCATION AND TO DISPLAY A COMMITMENT TO TRAFFIC AND PEDESTRIAN MANAGEMENT REPORTING AND REVIEWING, AN ON SITE RISK ASSESSMENT SHALL BE CONDUCTED PRIOR TO ERECTING ANY TRAFFIC CONTROL DEVICES.



**NOT TO SCALE**

**Location:**  
Suburb: Kiama  
1st Cross St: Terralong St  
2nd Cross St: N/A  
UBD Map Ref: N/A  
GPS/Charge: N/A

**Term:**  
Road Type: 2 Way  
Posted Speed: 40 km/h  
Operation: Road Closure  
Travelled Path: PAST  
AADT: >1000

**evolution**  
TRAFFIC CONTROL  
11 HENRYKOTE RD, MOSBAY NSW 2578 9388 881  
RME REGISTRATION CATEGORY: 9

PROVISED BY: **Jonathan Luna**  
PROF. CERT. NO: **440312978**  
TGS REF ID: **194202**  
PAGE: **01** OF **2**

**5.4 Road Closure - Belinda Street, Gerringong NSW 2534**

CSP Objective: Outcome 4.3: Our built environment is safe and inclusive; our infrastructure is well planned and well managed.

CSP Strategy: 4.3.2 Actively plan how we travel and create safe travel routes whether we are walking, cycling, driving or accessing public transport.

Delivery Program: 4.3.2.2 Implement road safety and traffic management programs and responsibilities

Item 5.4

**Summary**

Council has received an application for a Road Occupancy Permit for the full closure of Belinda Street, Gerringong between the Princes Highway (M1) Off Ramp and driveway entry to the Gerringong Train Station from 24 February 2024 to the 25 February 2024 between the hours of 12 p.m. Saturday to 12 p.m. Sunday (two days) to allow Sydney Trains conducting construction staging of the Belinda Street under bridge refurbishment.

**Financial implication**

Nil

**Risk implication**

The risk implications are minor as Council's permits process requires that the applicant maintains a \$20 Million public liability insurance policy for the duration of the work.

**Policy**

Section 138 of the *Roads Act 1993*.

**Consultation (internal)**

Manager Engineering & Technical Services

**Communication/Community engagement**

Kiama Local Traffic Committee

**Attachments**

- 1 TMP - Application for Road & Footpath Occupation Permit - Belinda Street Gerringong - Sydney Trains works 24/02 - 25/02/2024 - Platinum Traffic Services Pty Ltd

**Enclosures**

Nil

**RECOMMENDATION**

That the Committee endorses the temporary closure of Belinda Street, Gerringong between the Princes Highway (M1) Off Ramp and driveway entry to the Gerringong Train Station 24 February 2024 to the 25 February 2024 between the hours of 12 p.m. to 12 p.m. each day, subject to the applicant and suitably qualified traffic controllers complying with the following conditions:

Report of RMS delegated formal items


5.4 Road Closure - Belinda Street, Gerringong NSW 2534 (cont)

---

1. The applicant complies with the Traffic Management Plan shown in Attachments and conditions of consent imposed in the Road Occupation Permit by the Roads Authority.
2. Notification of the closures be given to Police, local emergency services, businesses and affected residents a minimum of 7 days prior to works occurring by the Applicant.
3. The closure be advertised on Council's website 7 days prior to the commencement of works.
4. Work dates and times delayed by inclement weather may be extended at the discretion and written approval of Kiama Council.
5. The Applicant receiving a Road Occupancy Permit from Transport for NSW for potential impacts on the M1 Princes Highway including messaging on the Highway variable message signage.
6. Specific engagement with the Ampol Service Station to minimise business impacts

### Background

Council, in its capacity as the Roads Authority, has received a Road Occupancy Permit application pursuant to Section 138 of the *Roads Act 1993*. The application is for the full road closure of Belinda Street Gerringong as shown on traffic management plan (between the Princes Highway (M1) Off Ramp and driveway entry to the Gerringong Train Station) for the scheduled maintenance of Sydney Trains bridge. The proposed dates 12 p.m. 24 February 2024 to the 12 p.m. 25 February 2024. The extent of work within the road includes using of loading crane to replace the existing beam and slabs of the overhead bridge. The road will be made trafficable at the end of day's work.

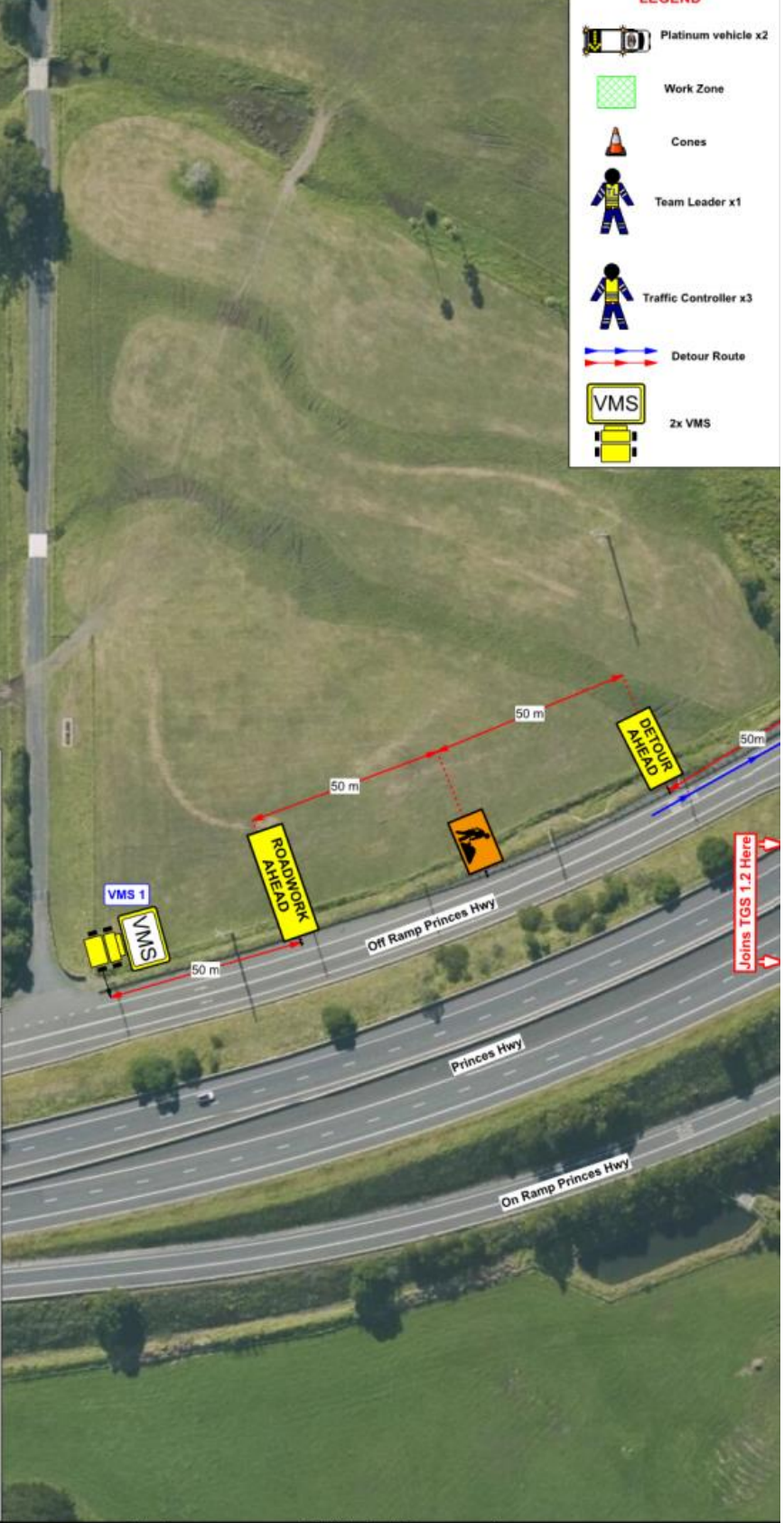


**Refer TGS 1.4 for Detour Route**

**Notes:**  
 1: Local constraints may not allow signage and devices to be placed in accordance with this TGS.  
 Signs and devices are to be positioned in accordance with tolerances shown in the TCAWS Manual Version 6.1 2022.  
 2: This TGS is suitable for Short term works.  
 3: This TGS is based on TNSW TGS recommendations from the TCAWS Manual Version 6.1 2022.  
 4: If not already noted, The existing speed limit is to be noted on this plan.  
 5: The value of speed limits displayed shall match the speed zone approval.  
 6: Ensure all approval requirements are met prior to commencing set up.  
 7: Cover all conflicting road signage where required.  
 8: If required cone spacing is to be no greater than 4m centres.  
 9: The site MUST comply with the TCAWS ( Traffic Control at Worksites ) Manual Version 6.1 2022.

**Amendments:**  
 All amendments to the TGS must be clearly documented on this plan. Amendments can only be made by the Traffic Control Supervisor holding a current PWZTMP card in consultation with the relevant project works supervisor.

Name: \_\_\_\_\_  
 PWZTMP Card Number: \_\_\_\_\_  
 Exp Date: \_\_\_\_\_  
 Date: \_\_\_\_\_ Sign: \_\_\_\_\_  
 Reason for modification: \_\_\_\_\_



**LEGEND**


- Platinum vehicle x2
- Work Zone
- Cones
- Team Leader x1
- Traffic Controller x3
- Detour Route
- 2x VMS

**VMS MESSAGE**  
To be displayed 2 weeks prior to closure

VMS 1	VMS 2
BELINDA ST	BELINDA ST
CLOSED	CLOSED
DATE (TBA)	DATE (TBA)


**VMS MESSAGE**  
To be displayed on the day of the closure

VMS 1	VMS 2
BELINDA ST	BELINDA ST
CLOSED	CLOSED
-	-
FOLLOW	FOLLOW
DETOUR	DETOUR
-	-
EXPECT	EXPECT
DELAYS	DELAYS
-	-



Revisions	No:	By:	Date:	Description:	Appr:
1	TM		02/11/23	issued for implementation	TM
2					
3					
4					
5					
6					

Original Size A3 1:1000

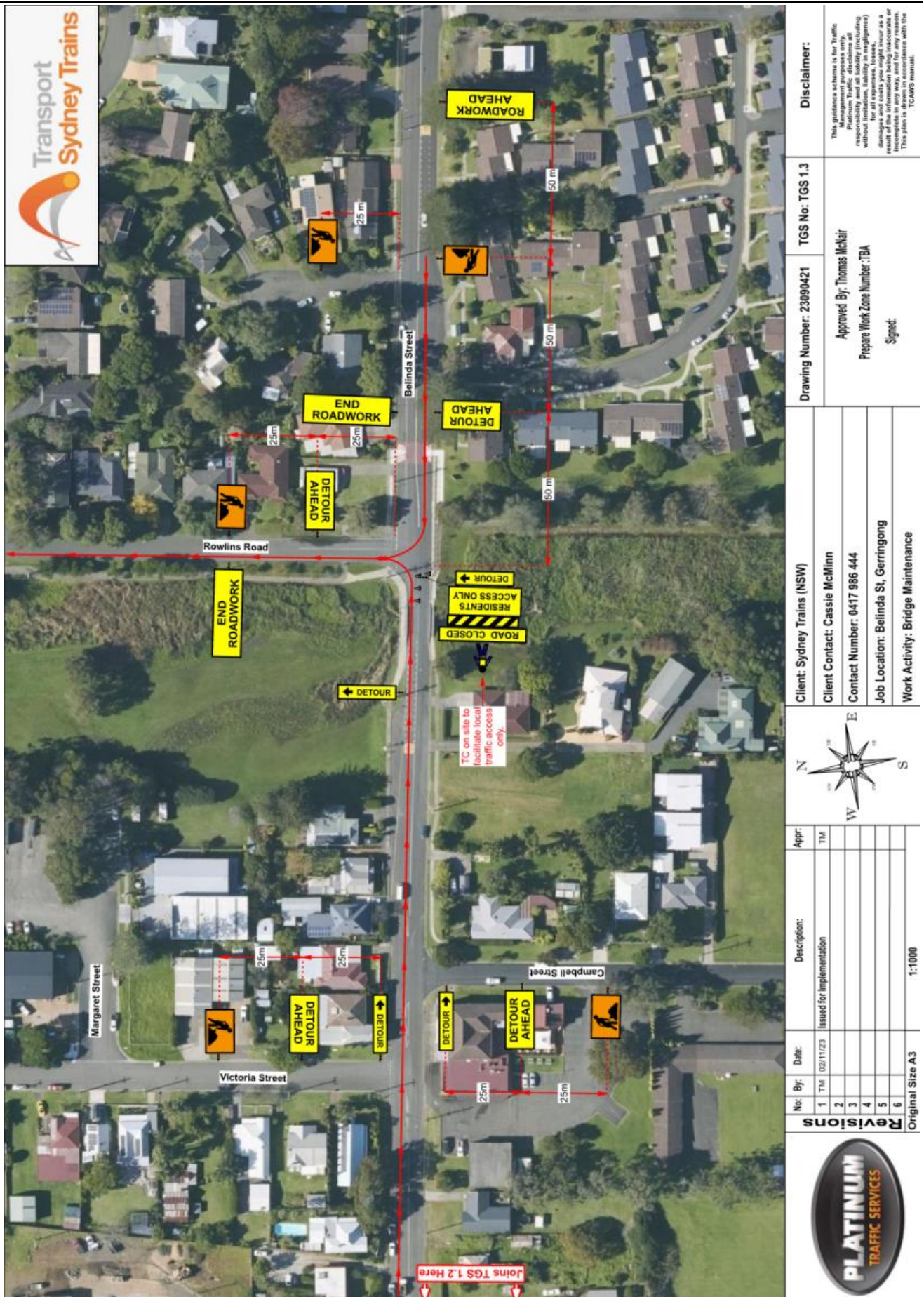


Client:	Sydney Trains (NSW)
Contact:	Cassio McMillin
Number:	0417 986 444
Location:	Belinda St, Gerringong
Activity:	Bridge Maintenance
Number:	2389421
TGS No:	TGS 1.1
Approved By:	Thomas McNair
PWZ Number:	TBA
Signed:	

**Disclaimer:**

This guidance scheme is for Traffic Management purposes only. Platinum Traffic disclaims all responsibility and all liability (including without limitation, liability in negligence) for all expenses, losses, damages and costs you might incur as a result of the information being inaccurate or incomplete in any way, and for any reason. This plan is drawn in accordance with the TCAWS manual.



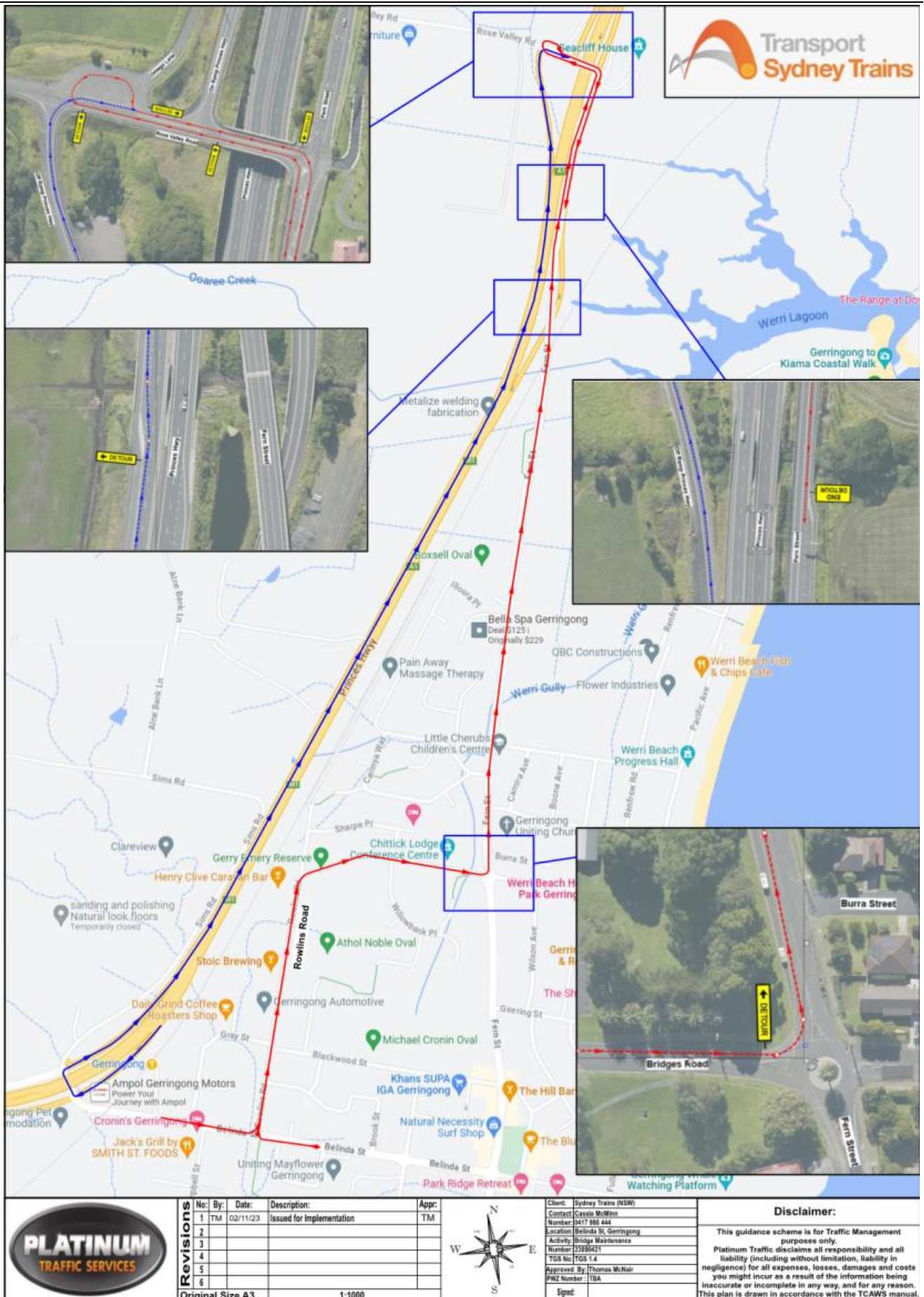


No.	By	Date	Description	Appr.
1	TM	02/11/23	Issued for Implementation	TM
2				
3				
4				
5				
6				

Original Size A3 1:1000

Client: Sydney Trains (NSW)	Client Contact: Cassie McMinn	Client: Sydney Trains (NSW)	TGS No: TGS 1.3
Contact Number: 0417 986 444	Job Location: Belinda St, Gerringong	Approved By: Thomas McKair	Drawing Number: 23090421
Work Activity: Bridge Maintenance	Prepare Work Zone Number: TBA	Signed:	Disclaimer:

This guidance scheme is for Traffic Management. Platinum Traffic Services Pty Ltd is not responsible for any damage or loss of property or injury to persons or animals without limitation, liability or negligence. This plan is drawn in accordance with the TCAMS manual.



Item 5.4

Attachment 1

**5.5 Terralong Street Underbridge - Replacement Collision Protection Beams**

CSP Objective: Outcome 11: We are well connected within our local community, and to our region

CSP Strategy: 11.1 Actively plan how we travel and create safe travel routes whether we are walking, cycling, driving or accessing public transport

Delivery Program: 11.1.2 Implement road safety and traffic management programs and responsibilities

Item 5.5

**Summary**

Transport for NSW (Sydney Trains) have provided information for the proposed replacement of the bridge protection infrastructure for the Illawarra Rail Line bridge over Terralong Street. The report seeks initial engineering advice from the LTC to provide to the proponent.

**Financial implication**

Nil

**Risk implication**

Improved collision protection for the rail bridge.

Loss of a single parking space to facilitate the works.

Narrowing of the lane widths due to the construction of a central median island to facilitate the works.

Potential right turn implications for larger vehicles into Railway Parade.

**Policy**

Nil

**Consultation (internal)**

Manager Engineering & Technical Services

**Communication/Community engagement**

Kiama Local Traffic Committee.

**Attachments**

- 1 TRANSPORT NSW - SYDNEY TRAINS - Terralong Street Underbridge - Replacement Collision Protection Beams - Drawings - PS114109-ST-DRG-COMBINED

**Enclosures**

Nil

**RECOMMENDATION**

That the Committee recommends that the proponent provides:



Report of RMS delegated formal items

5.5 Terralong Street Underbridge - Replacement Collision Protection Beams  
(cont)

---

1. A risk assessment for potential collisions with the infrastructure within a 40km/h speed zone and assess if additional protection is required in relation to the columns.
2. Turning path information in relation to heavy vehicle movements in the vicinity of the works.
3. Details in relation to the resulting traffic lane widths in relation to the proposal.
4. Details in relation to the proposed signage adjustments that will be required as a result of the works.

Item 5.5

### Background

Council is in receipt of drawings from Transport for NSW (Sydney Trains) providing proposed details for modifications in Terralong Street, Kiama to provide new Collision Protection Beam infrastructure for the Illawarra Rail line bridge.

Details are included in the attached plans and involve the construction of columns adjacent to the roadway to support collision protection beams on the approaches to the bridge protect from impacts by over height vehicles.

As part of the proposal, it is intended to:

1. Adjust the kerb on the southern side of the road to provide space for the construction of one of the support columns, which will result in the loss of one (1) parking space;
2. Construction of a raised centre median island to permit the construction of two support columns, which will result in the loss of lane width in the vicinity, with the exact impacts still to be quantified by the proponent;
3. Associated adjustments to existing signage as a result of the proposed works.

At this stage, these drawings are provided for the information of the traffic committee, and the proposed works will be subject to relevant regulatory approvals prior to proceeding.

The works are currently scheduled for construction starting at the end of Q1 2024.



# KIAMA

## ILLAWARRA LINE 118.975km

### TERRALONG STREET UNDERBRIDGE REPLACEMENT COLLISION PROTECTION BEAMS

#### REFERENCE DOCUMENTS

- ELECTRONIC SURVEY PROVIDED BY HARD & FORESTER  
DRAWING 15A/0001 - DETAILED SURVEY TERRALONG STREET UNDERBRIDGE REV 00
- UTILITY PLAN PROVIDED BY DURRIN CONSTRUCTION PTY LTD - DRAWING DTPS6-UT-01
- GEOTECHNICAL REPORT IN GEOTECHNICAL INVESTIGATION REPORT 228H42/01
- EXISTING BRIDGE DRAWINGS NSW RAILWAYS (CONTRACT No 0  
SHEM BRIDGE OVER TERRALONG STREET  
DRAWING No 1 AND DRAWING No 3
- EXISTING BRIDGE DRAWINGS IRWIN JOHNSON & PARTNERS  
REPLACEMENT OF TERRALONG STREET UNDERBRIDGE  
DRAWING CV 108109 TO CV 108107A

#### SCHEDULE OF DRAWINGS

- HOLD**
- PS114109-ST-DRG-001 COVER SHEET
  - PS114109-ST-DRG-002 NOTES SHEET
  - PS114109-ST-DRG-000 GENERAL ARRANGEMENT - SHEET A
  - PS114109-ST-DRG-009 PILE DETAILS - SHEET A
  - PS114109-ST-DRG-029 PILE DETAILS - SHEET B
  - PS114109-ST-DRG-021 COLLISION PROTECTION BEAM AND END FRAME DETAILS - SHEET A
  - PS114109-ST-DRG-040 COLLISION PROTECTION BEAM AND END FRAME DETAILS - SHEET B
  - PS114109-ST-DRG-041 COLLISION PROTECTION BEAM AND END FRAME DETAILS - SHEET C
  - PS114109-ST-DRG-050 UTILITIES PLAN



LOCALITY PLAN

DRAWING COLOUR CODED - PRINT ALL COPIES IN COLOUR

NOT FOR CONSTRUCTION

DATE	BY	DATE
DRAWING CHECK PRINT		
DRAWING CHECK		
ENGINEERING CHECK		
BACKCHECKING CHECK		
NEXT ACTION		

NO	DESCRIPTION	DATE	STATUS	APPROVED	DESIGNED	DRAWN	CHECKED	DATE
2	95% CRITICAL DESIGN REVIEW	14.08.22	IN PROGRESS					14.08.22
1	90% DETAILED DESIGN	14.08.22	COMPLETE					14.08.22
1	95% CRITICAL DESIGN REVIEW	14.08.22	COMPLETE					14.08.22

NO	DESCRIPTION	DATE	STATUS	APPROVED	DESIGNED	DRAWN	CHECKED	DATE
2	95% CRITICAL DESIGN REVIEW	14.08.22	COMPLETE					14.08.22
1	90% DETAILED DESIGN	14.08.22	COMPLETE					14.08.22
1	95% CRITICAL DESIGN REVIEW	14.08.22	COMPLETE					14.08.22

NO	DESCRIPTION	DATE	STATUS	APPROVED	DESIGNED	DRAWN	CHECKED	DATE
2	95% CRITICAL DESIGN REVIEW	14.08.22	COMPLETE					14.08.22
1	90% DETAILED DESIGN	14.08.22	COMPLETE					14.08.22
1	95% CRITICAL DESIGN REVIEW	14.08.22	COMPLETE					14.08.22

**KIAMA**  
 ILLAWARRA LINE 118.975km  
 TERRALONG STREET UNDERBRIDGE  
 REPLACEMENT COLLISION PROTECTION BEAMS  
 COVER SHEET

STATUS: 95% CRITICAL DESIGN REVIEW  
 SHEET: 1 OF 10  
 FILE NO: PS114109-ST-DRG-001  
 PROJECT: PS114109-ST-DRG-001  
 DRAWING NO: TBC

**GENERAL NOTES**

**GENERAL**  
 1. DIMENSIONS SHOWN ON THESE DRAWINGS SHALL BE VERIFIED BY THE CONTRACTOR ON SITE. THESE DRAWINGS SHALL NOT BE SCALED FOR DIMENSIONS.

**DESIGN**  
 1. DESIGN CODES  
 - AS 5000-2017 BRIDGE DESIGN SAFT  
 - AS 2700-2017 BRIDGE DESIGN WIND LOADS  
 - AS 2700-2017 BRIDGE DESIGN COLLISION PROTECTION  
 2. DESIGN IMPACT LOAD ON COLLISION PROTECTION BEAMS

LOADS	ULTIMATE LIMIT STATE
(A) HORIZONTAL LOAD (kN)	1000
(B) VERTICAL LOAD (kN) (UP/LIFT)	500

LOADS	ULTIMATE LIMIT STATE
(D) HORIZONTAL LOAD IN ANY DIRECTION (kN)	2700

LOADS (A), (B) AND (D) NOT APPLIED CONCURRENTLY  
 3. OTHER DESIGN REFERENCES  
 (A) JK GEOTECHNICAL INVESTIGATION REPORT 228/2014/04  
 (B) PS114109-STR-REP-001 REV. A, WSP DESIGN REPORT  
 4. DESIGN LIFE  
 - SUBSTRUCTURE (PILES, COLUMNS) 120 YEARS  
 - COLLISION PROTECTION BEAMS, FITTINGS 25 YEARS  
 - PROTECTIVE COATINGS SYSTEM FOR STEELWORK

**CONSTRUCTION**

1. ALL LIFTING DEVICES SHALL BE DESIGNED AND CERTIFIED BY THE CONTRACTOR, AND SHALL BE ACCEPTABLE TO THE WORKCOVERS AUTHORITY OF NSW.  
 2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UTILITIES, DAMAGED DRAINAGE, AND LIFTING OF ANY EXISTING OR BURIED UTILITIES OR DAMAGE OCCURS. IT SHALL BE REPAIRED BY THE CONTRACTOR IN ACCORDANCE WITH THE SPECIFICATION.  
 3. FULL DETAILS OF THE CONTRACTOR'S PROPOSED LIFTING DEVICES, METHODS AND EQUIPMENT SHALL BE SUBMITTED TO THE PRINCIPAL FOR REVIEW PRIOR TO GROUND DURING CONSTRUCTION. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO ENSURE THAT THE STRUCTURES INCLUDING THE RAILWAY BRIDGE ARE MAINTAINED IN A SAFE AND STABLE CONDITION AT ALL TIMES, AND THAT NO PART BECOMES OVERSTRESSED. THE CONTRACTOR SHALL DEVELOP WORK METHOD STATEMENTS FOR ALL LIFTING OPERATIONS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UTILITIES SUCH AS BRACING, PROPPING AND SHORING ETC. TO KEEP THE WORKS STABLE AT ALL TIMES. THE CONTRACTOR IS TO ENGAGE A QUALIFIED STRUCTURAL ENGINEER TO DESIGN AND CERTIFY THE TEMPORARY WORKS.  
**MEASURED CLEARANCE**  
 1. BRIDGE CLEARANCE HEIGHT AND COLLISION PROTECTION BEAM LEVEL IN ACCORDANCE WITH THE SPECIFICATION SHALL BE MEASURED PRIOR TO CONSTRUCTION OF THE BEAM. THE MEASURED CLEARANCE UNDER THE COLLISION PROTECTION BEAM SHALL BE 3m EASTBOUND AND 3m WEST BOUND, MEASURED FROM ROAD SURFACE TO UNDERSIDE OF BEAM TAPER PLATE.  
 2. DESIGN ASSUMES A VERTICAL CLEARANCE TO THE BRIDGE OF 3.6m. THIS SHALL BE CONFIRMED PRIOR TO CONSTRUCTION. CONCERN THE MEASUREMENTS, ADVISE THE ASSET AUTHORITY AND ALTER THE STORAGE AS APPROPRIATE.  
**ROAD CLEARANCE SIGN**  
 1. SIZE LETTERING FORMAT BASICALLY TO TNSW STANDARD. INSTALLATION AND MANUFACTURE OR PURCHASE TO BE ARRANGED BY SYDNEY TRAINS. 7.0m CLEARANCE 3.4m TO BE DISPLAYED ON BOTH CLEARANCE SIGNS AT THE BRIDGE.

**STRUCTURAL STEELWORK**

1. ALL WORKMANSHIP AND MATERIALS TO BE IN ACCORDANCE WITH AS 4100 AND TNSW SPECIFICATION 801 AND T H R O 1000 ST CIVIL INFRASTRUCTURE CONSTRUCTION APPENDIX A.  
 2. EXPOSURE ENVIRONMENT / CLASSIFICATIONS TO AS 4312, AS2509.  
 3. PROTECTIVE COATINGS TO BE CATEGORICAL CS.  
 4. PROTECTIVE COATINGS TO BE CATEGORICAL CS.  
 5. PROTECTIVE COATINGS TO BE CATEGORICAL CS.  
 6. ALL HOT-ROLLED STEEL SECTIONS MUST BE GRADE 350 IN ACCORDANCE WITH AS/NZS 3678 UNLESS OTHERWISE SPECIFIED.  
 7. ALL STEEL PLATES MUST BE GRADE 350 IN ACCORDANCE WITH AS/NZS 3678 UNLESS OTHERWISE SPECIFIED.  
 8. ALL BOLTS, NUTS AND WASHERS ARE TO BE SUPPLIED AND INSTALLED IN ACCORDANCE WITH AS 4100 AND TNSW SPECIFICATION B A 0 AND MUST BE BOLTED TO DETAILS ARE SPECIFIED AS FOLLOWS:  
 - USING A STANDARD WRENCH TO TIGHTEN TO CLASS 8.8 TO AS 1111, TIGHTENED TO 8.8/5 - DENOTES HIGH STRENGTH BOLTS OF STRENGTH GRADE 8.8 TO AS 1152, TIGHTENED USING A STANDARD WRENCH TO A SNAG TIGHT CONDITION.  
 9. ALL WELDING TO BE CARRIED OUT AND INSPECTED IN ACCORDANCE WITH AS/NZS 1554.1 AND THE PROJECT SPECIFICATION WITH ADDITIONAL REQUIREMENTS AS NOTED ON DRAWINGS.  
 10. ALL WELDS ARE TO BE FULL PENETRATION BUTT WELDS UNLESS SPECIFIED OTHERWISE.  
 11. ALL EXPOSED BUTT WELDS ARE TO BE GROUND FLUSH.  
 12. EDGES TO BE PROTECTIVE TREATED TO BE FOUNDED TO A RADIUS OF 2 mm.  
 13. ALL WELDS TO BE CARRIED OUT AND INSPECTED IN ACCORDANCE WITH AS/NZS 1554.1 AND THE PROJECT SPECIFICATION WITH ADDITIONAL REQUIREMENTS AS NOTED ON DRAWINGS.  
 14. THE WELD METAL DESIGNATION FOR ALL WELDS MUST BE E49XX.  
 15. WELDING SYMBOLS COMPLY WITH AS 1013.  
 16. STEELWORK TO BE CARRIED AS NOTED ON THE DRAWINGS. MANUFACTURING TOLERANCES MUST BE ACCOUNTED FOR BY THE CONTRACTOR.  
 17. THE CONTRACTOR MUST PROVIDE ALL CLEATS AND DRILL ALL HOLES NECESSARY FOR JOINING STEEL, CONCRETE AND OTHER ELEMENTS TO STEEL WHETHER OR NOT DETAILED ON THE DRAWINGS.  
 18. ALL DRILLING OPERATIONS ARE TO BE CARRIED OUT IN THE COLOR THIS SUBSEQUENT TO THE WELDING OPERATIONS AND TO BE CARVED MARKINGS MUST BE PAINTED ON THE FRONT FACE IN ACCORDANCE WITH T H R O 1202/ST.  
 19. COLOR OF THE PAINT FINISH FOR THE SUPPORT COLUMN AND END FRAME TO BE DETERMINED IN CONSULTATION WITH KIAMA COUNCIL.  
 20. PAINT FINISH SYSTEM FOR ALL STEEL ELEMENTS ABOVE THE GROUND, INCLUDING IN ACCORDANCE WITH AS 2321.1, END FRAMES AND COLUMNS TO BE PMS OR P8B.  
 21. ALL WELDS AND CORNERS AROUND DRILLED HOLES SHOULD HAVE A STRIPE COAT BEFORE THE MAIN APPLICATION WORKS.  
 22. CHAIN L.B. SHACKLES  
 1. CHAIN L.B. SHACKLES TO BE PMS GRADE 60 OR APPROVED EQUIVALENT.  
 2. SIZE IS TO BE 10 TONNES LENGTH TO SUIT.  
 3. SHACKLES SHALL BE GRADE 'M' STEEL SHACKLES BODY DIAMETER 20mm.  
 4. SWL 3 TONNES, SCREW PIN  
**UTILITIES**  
 1. EXISTING UTILITIES ARE SHOWN INDICATIVELY ON DRAWING NO. 70 AND 71.  
 2. ALL UTILITIES TO BE RELOCATED PRIOR TO CONSTRUCTION OF THE COLLISION PROTECTION BEAM.  
 3. CONFIRMATION OF TEMPORARY WORKS AND REQUIRED UTILITIES RELOCATION IS THE RESPONSIBILITY OF THE CONTRACTOR. THE EXTENT OF RELOCATION IS SUBJECT TO THE FINAL TEMPORARY WORKS DESIGN, INCLUDING SUFFICIENT WORKING ROOM AROUND THE STRUCTURE.  
 4. ALL UTILITIES TO BE RELOCATED PRIOR TO UTILITIES ARE PRESENTED WITHIN WSP REPORT PS114109-STR-REP-001.

**CONCRETE**

1. CONCRETE MUST BE SUPPLIED AND PLACED IN ACCORDANCE WITH CONCRETE EXPOSURE CLASSIFICATION.  
 2. PILE AND COLUMN INFILL.  
 3. MINIMUM 28 DAY COMPRESSIVE STRENGTH OF CONCRETE MUST BE - A0 PMS.  
 4. PILE AND COLUMN INFILL SHALL HAVE - A0 PMS.  
 5. MINIMUM ULTIMATE DIFFUSION COEFFICIENT OF 2.0E-12 m<sup>2</sup>/sec

**REINFORCEMENT**

1. NORMAL COVER TO REINFORCEMENT NEAREST TO THE CONCRETE SURFACE SHALL BE:  
 - PILES AND COLUMNS - 50mm UNLESS SPECIFIED OTHERWISE  
 - UNLESS OTHERWISE SPECIFIED, THE MINIMUM LENGTHS OF LAPS SHALL BE:  
 PILE AND COLUMN  

BAR SIZE	N12	N14	N16
a) HORIZONTAL BARS WITH >90deg OF CONCRETE CAST BELOW BAR	450	1100	1500
b) OTHER BARS	350	750	1100

 2. LAPS NOT SHOWN ON THE DRAWINGS SHALL BE STAGGERED SO THAT NO MORE THAN 50% OF THE BARS ARE LAPPED IN ANY CROSS SECTION.  
 3. UNLESS OTHERWISE SPECIFIED, REINFORCEMENT SHALL BE GRADE B500 IN ACCORDANCE WITH AS/NZS 4671.  
 4. ABBREVIATIONS:  
 ES - EQUALLY SPACED  
 BAR BENDING AND HOOK DETAILS SHALL BE IN ACCORDANCE WITH TNSW STANDARD DRAWING 8103.  
 5. BAR SHAFR CORES WHERE USED ARE IN ACCORDANCE WITH TNSW STANDARD DRAWING 8103.

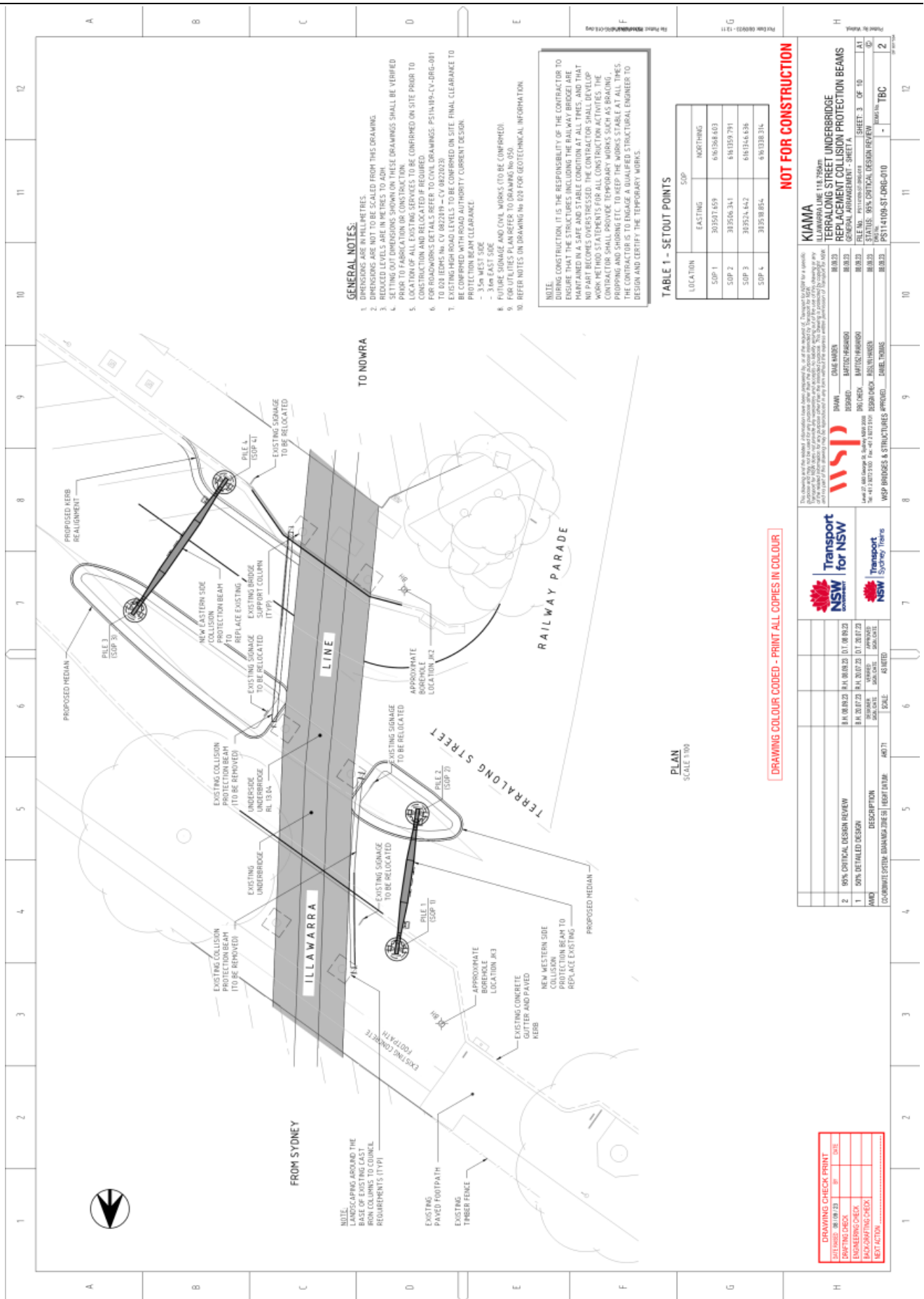
DATE	BY	DATE
20/09/23	SP	
20/09/23	SP	
20/09/23	SP	
20/09/23	SP	
20/09/23	SP	

**NOT FOR CONSTRUCTION**

**Transport for NSW**  
 NSW Sydney Trains

**WSP**  
 WSP BRIDGES & STRUCTURES APPROVED

**PROJECT INFORMATION**  
 PROJECT: TERRALONG STREET UNDERBRIDGE REPLACEMENT COLLISION PROTECTION BEAMS  
 SHEET: 2 OF 10  
 FILE NO: PS114109-ST-DRG-COMBINED  
 STATUS: 95% CRITICAL DESIGN REVIEW  
 DRAWN BY: DANIEL THOMAS  
 CHECKED BY: DANIEL THOMAS  
 DESIGNED BY: DANIEL THOMAS  
 APPROVED BY: DANIEL THOMAS  
 PROJECT NO: PS114109-ST-DRG-COMBINED



**GENERAL NOTES:**

1. DIMENSIONS ARE NOT TO BE SCALED FROM THIS DRAWING.
2. DIMENSIONS ARE IN METRES TO ADM.
3. REDUCED LEVELS ARE IN METRES TO ADM.
4. SETTING OUT DIMENSIONS SHOWN ON THESE DRAWINGS SHALL BE VERIFIED PRIOR TO FABRICATION OR CONSTRUCTION.
5. LOCATION OF ALL EXISTING SERVICES TO BE CONFIRMED ON SITE PRIOR TO CONSTRUCTION AND RELOCATED IF REQUIRED.
6. FOR ROADWORKS DETAILS REFER TO CIVIL DRAWINGS PS114109-CV-DRG-001 TO 004 UNDER NO. CO 8022019 - CV 0020209.
7. ALL WORK TO BE COMPLETED ON SITE FINAL CLEARANCE TO BE CONFIRMED WITH ROAD AUTHORITY CURRENT DESIGN.
8. PROTECTION BEAM CLEARANCE:
  - 3.5m WEST SIDE
  - 3.0m EAST SIDE
9. FOR UTILITIES PLANNER REFER TO DRAWING NO 050.
10. REFER NOTES IN DRAWING NO 020 FOR GEOTECHNICAL INFORMATION.

**NOTE:**  
 DURING CONSTRUCTION IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO ENSURE THAT THE STRUCTURES INCLUDING THE RAILWAY BRIDGE ARE MAINTAINED IN A SAFE AND STABLE CONDITION AT ALL TIMES, AND THAT NO PART BECOMES OVERSTRESSED. THE CONTRACTOR SHALL DEVELOP WORK METHOD STATEMENTS FOR ALL CONSTRUCTION ACTIVITIES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY APPROVALS AND PERMITS FOR THE WORKS. STABILISATION OF EXISTING STRUCTURES TO BE ENGAGED BY A QUALIFIED STRUCTURAL ENGINEER TO DESIGN AND CERTIFY THE TEMPORARY WORKS.

**TABLE 1 - SETOUT POINTS**

LOCATION	EASTING	NORTHING
SOP 1	303507.659	616158.403
SOP 2	303506.341	616159.791
SOP 3	303514.642	616154.636
SOP 4	303510.854	616153.314

**NOT FOR CONSTRUCTION**

**DRAWING COLOUR CODED - PRINT ALL COPIES IN COLOUR**

**PLAN SCALE 1:100**

NO.	DATE	DESCRIPTION	BY	CHECKED	SCALE	AS NOTED
2	01/08/23	95% CRITICAL DESIGN REVIEW	AM/D	AM/D	AS NOTED	
1	01/08/23	90% DETAILED DESIGN	AM/D	AM/D	AS NOTED	

NO.	DATE	DESCRIPTION	BY	CHECKED	SCALE	AS NOTED
1	01/08/23	90% DETAILED DESIGN	AM/D	AM/D	AS NOTED	
2	01/08/23	95% CRITICAL DESIGN REVIEW	AM/D	AM/D	AS NOTED	

**Transport for NSW**  
 NSW  
 Transport  
 Sydney Trains

**WSP**  
 WSP BRIDGES & STRUCTURES APPROVED

**TABLE 1 - SETOUT POINTS**

LOCATION	EASTING	NORTHING
SOP 1	303507.659	616158.403
SOP 2	303506.341	616159.791
SOP 3	303514.642	616154.636
SOP 4	303510.854	616153.314

**NOT FOR CONSTRUCTION**



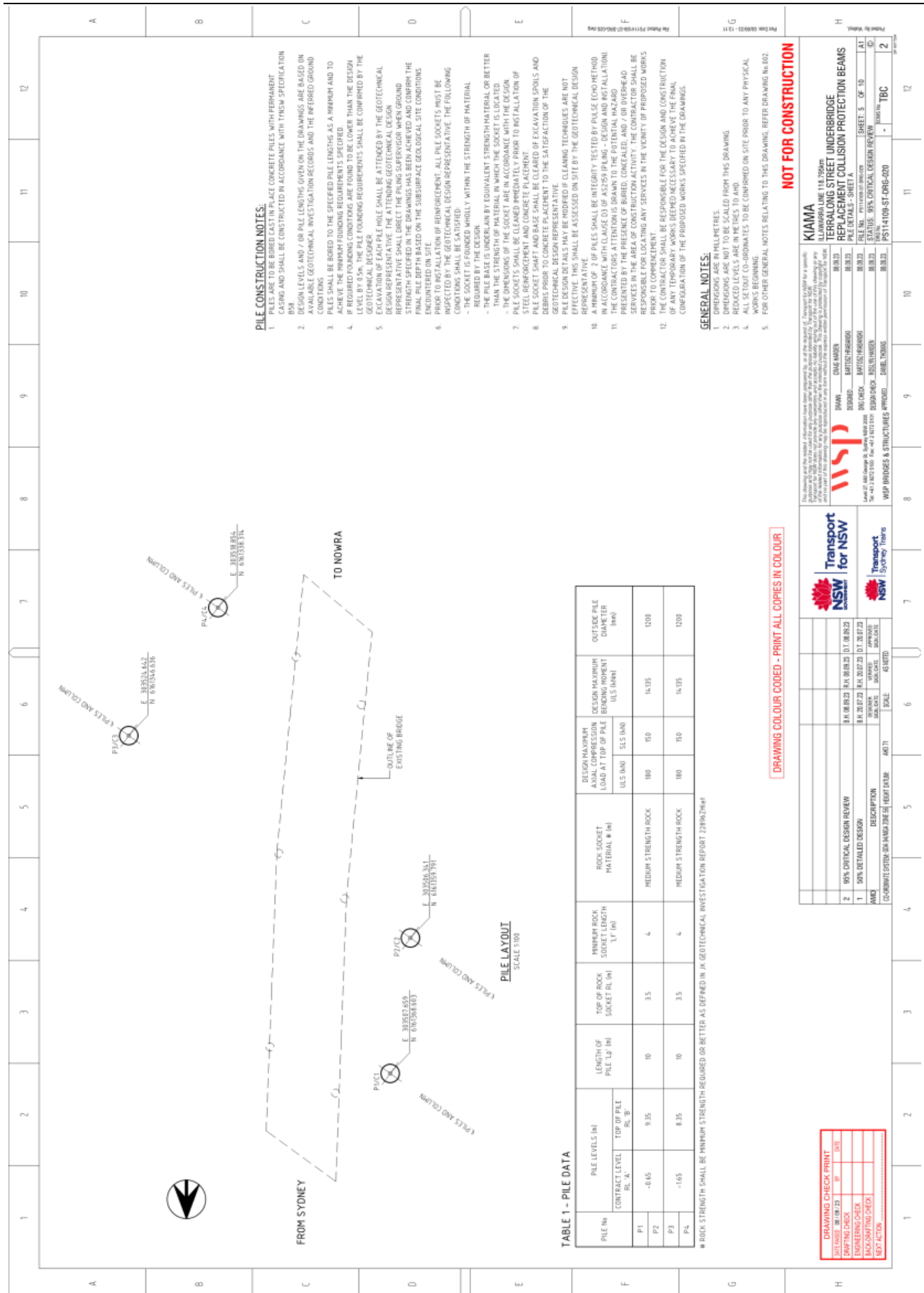


TABLE 1 - PILE DATA

PILE No	PILE LEVELS (M)		LENGTH OF PILE (M)	TOP OF ROCK SOCKET (M)	MINIMUM ROCK SOCKET LENGTH (M)	ROCK SOCKET MATERIAL #	DESIGN MAXIMUM AXIAL COMPRESSION LOAD AT TOP OF PILE (kN)	DESIGN MAXIMUM BENDING MOMENT (kNm)		OUTSIDE PILE DIAMETER (mm)
	CONTRACT LEVEL (M)	TOP OF PILE (M)						ULS (kNm)	SLS (kNm)	
P1	-0.65	9.35	10	3.5	4	MEDIUM STRENGTH ROCK	180	150	14.05	1200
P2										
P3										
P4	-1.65	8.35	10	3.5	4	MEDIUM STRENGTH ROCK	180	150	14.05	1200

# ROCK STRENGTH SHALL BE MINIMUM STRENGTH REQUIRED OR BETTER AS DEFINED IN JK GEOTECHNICAL INVESTIGATION REPORT 22896Z#4

- PILE CONSTRUCTION NOTES:**
- PILES ARE TO BE BORED CAST IN PLACE CONCRETE PILES WITH PERMANENT CASING AND SHALL BE CONSTRUCTED IN ACCORDANCE WITH THEIR SPECIFICATION BSH.
  - DESIGN LEVELS AND 7.0M PILE LENGTHS GIVEN ON THE DRAWINGS ARE BASED ON AVAILABLE GEOTECHNICAL INVESTIGATION RECORDS AND THE UNPROBABLE GROUND.
  - PILES SHALL BE BORED TO THE SPECIFIED PILE LENGTHS AS A MINIMUM AND TO ACHIEVE THE MINIMUM FOUNDING REQUIREMENTS SPECIFIED.
  - IF REQUIRED FOUNDING CONDITIONS ARE FOUND TO BE LOWER THAN THE DESIGN LEVEL BY 0.5M, THE PILE FOUNDING REQUIREMENTS SHALL BE CONFIRMED BY THE GEOTECHNICAL DESIGNER.
  - EXCAVATION OF EACH PILE HOLE SHALL BE ATTENDED BY THE GEOTECHNICAL DESIGN REPRESENTATIVE. THE ATTENDING GEOTECHNICAL DESIGN REPRESENTATIVE SHALL BE PRESENT THROUGHOUT THE PILING OPERATION AND CONDUCT THE FINAL PILE DEPTH CHECKS AND CONFIRM THE FINAL PILE DEPTH BASED ON THE SUBSURFACE GEOLOGICAL SITE CONDITIONS ENCOUNTERED ON SITE.
  - PRIOR TO INSTALLATION OF REINFORCEMENT, ALL PILE SOCKETS MUST BE INSPECTED BY THE GEOTECHNICAL DESIGN REPRESENTATIVE. THE FOLLOWING CONDITIONS SHALL BE SATISFIED:
    - THE SOCKET IS FOUNDED WHOLLY WITHIN THE STRENGTH OF MATERIAL.
    - THE PILE IS CLEAN BY EQUIVALENT STRENGTH MATERIAL OR BETTER.
    - THE STRENGTH OF MATERIAL IN WHICH THE SOCKET IS LOCATED.
    - THE DIMENSIONS OF THE SOCKET ARE IN ACCORDANCE WITH THE DESIGN.
  - PILE SOCKETS SHALL BE CLEANED IMMEDIATELY PRIOR TO INSTALLATION OF STEEL REINFORCEMENT AND CONCRETE PLACEMENT.
  - PILE SOCKET SHAFT AND BASE SHALL BE CLEARED OF EXCAVATION SPILLS AND DEBRIS PRIOR TO CONCRETE PLACEMENT TO THE SATISFACTION OF THE GEOTECHNICAL DESIGN REPRESENTATIVE.
  - ALL PILING OPERATIONS AND CLEANING TECHNIQUES ARE NOT EFFECTIVE. THIS SHALL BE ASSESSED ON SITE BY THE GEOTECHNICAL DESIGN REPRESENTATIVE.
  - A MINIMUM OF 2 OF PILES SHALL BE INTEGRITY TESTED BY PULSE ECHO METHOD IN ACCORDANCE WITH CLAUSE D3 OF AS2999 PILING - DESIGN AND INSTALLATION. THE CONTRACTORS ATTENTION IS DRAWN TO THE POTENTIAL HAZARD PRESENTED BY THE PRESENCE OF BURIED, CONCEALED, AND / OR OVERHEAD SERVICES IN THE AREA OF CONSTRUCTION ACTIVITY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CLEARING ANY SERVICES IN THE VICINITY OF PROPOSED WORKS PRIOR TO CONSTRUCTION.
  - THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE DESIGN AND CONSTRUCTION OF ANY TEMPORARY WORKS DEEMED NECESSARY TO ACHIEVE THE FINAL CONFIGURATION OF THE PROPOSED WORKS SPECIFIED IN THE DRAWINGS.

**GENERAL NOTES:**

- DIMENSIONS ARE IN MILLIMETRES.
- DIMENSIONS ARE NOT TO BE SCALED FROM THIS DRAWING.
- REDUCED LEVELS ARE IN METRES TO AHD.
- ALL SETOUT CO-ORDINATES TO BE CONFIRMED ON SITE PRIOR TO ANY PHYSICAL WORKS BEGINNING.
- FOR OTHER GENERAL NOTES RELATING TO THIS DRAWING, REFER DRAWING IN 802.

**NOT FOR CONSTRUCTION**

**DRAWING COLOUR CODED - PRINT ALL COPIES IN COLOUR**

**Transport for NSW**  
**NSW**  
 Sydney Trains

**WSP**  
 WSP BRIDGES & STRUCTURES APPROVED

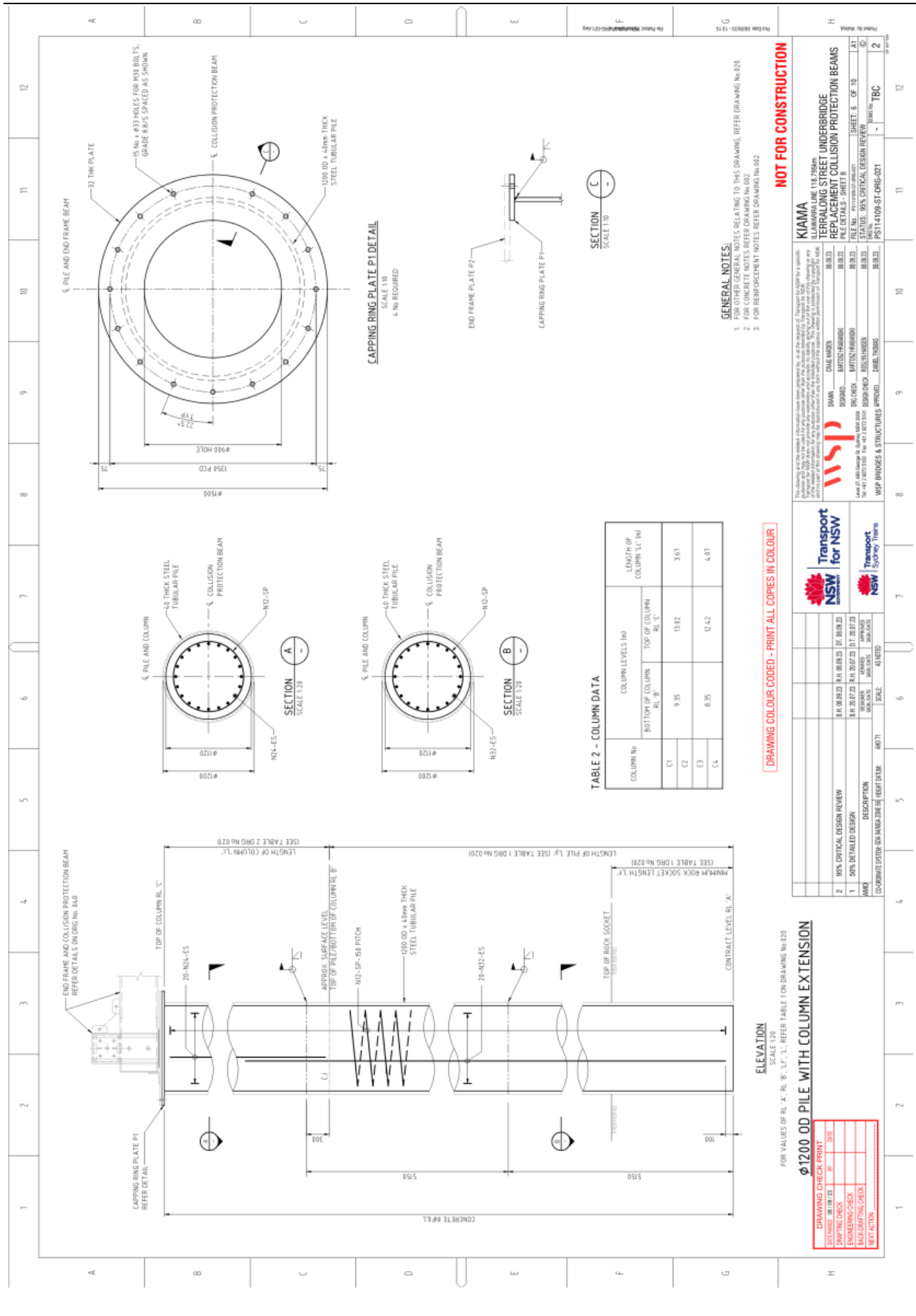
**Project Information:**  
 PROJECT: TERRALONG STREET UNDERBRIDGE REPLACEMENT COLLISION PROTECTION BEAMS  
 SHEET: 5 OF 10  
 FILE NO: PS114109-ST-DRG-C001  
 STATUS: 95% CRITICAL DESIGN REVIEW  
 PROJECT MANAGER: DANIEL THOMAS  
 PROJECT ENGINEER: DANIEL THOMAS  
 PROJECT SUPERVISOR: DANIEL THOMAS

**Revision History:**

NO.	DESCRIPTION	DATE
1	ISSUED FOR CONSTRUCTION	10/08/23
2	ISSUED FOR CONSTRUCTION	10/08/23

**Approval:**  
 APPROVED: DANIEL THOMAS  
 PROJECT ENGINEER

DRAWING CHECK PRINT	
DATE CHECKED: 08/08/23	DATE: 08/08/23
DRAWING CHECK: [Signature]	BY: [Signature]
ENGINEERING CHECK: [Signature]	DATE: 08/08/23
ACCOUNTING CHECK: [Signature]	DATE: 08/08/23
NEXT ACTION:	



**GENERAL NOTES:**  
 1. FOR OTHER GENERAL NOTES RELATING TO THIS DRAWING, REFER DRAWING No. 629.  
 2. FOR REINFORCEMENT DETAILS REFER DRAWING No. 629.  
 3. FOR REINFORCEMENT NOTES REFER DRAWING No. 622.

**NOT FOR CONSTRUCTION**

**DRAWING COLOUR CODED - PRINT ALL COPIES IN COLOUR**

**KIAMA**  
 TERRALONG STREET UNDERBRIDGE  
 REPLACEMENT COLLISION PROTECTION BEAMS  
 FILE NO. PS114109-ST-DRG-021  
 SHEET: 6 OF 10  
 STATUS: 95% CRITICAL DESIGN REVIEW  
 DRAWN: DAME HARDY  
 CHECKED: SMITH/FRANKHO  
 DATE: 20/07/23  
 WSP BRIDGES & STRUCTURES APPROVED: DAVID THOMAS

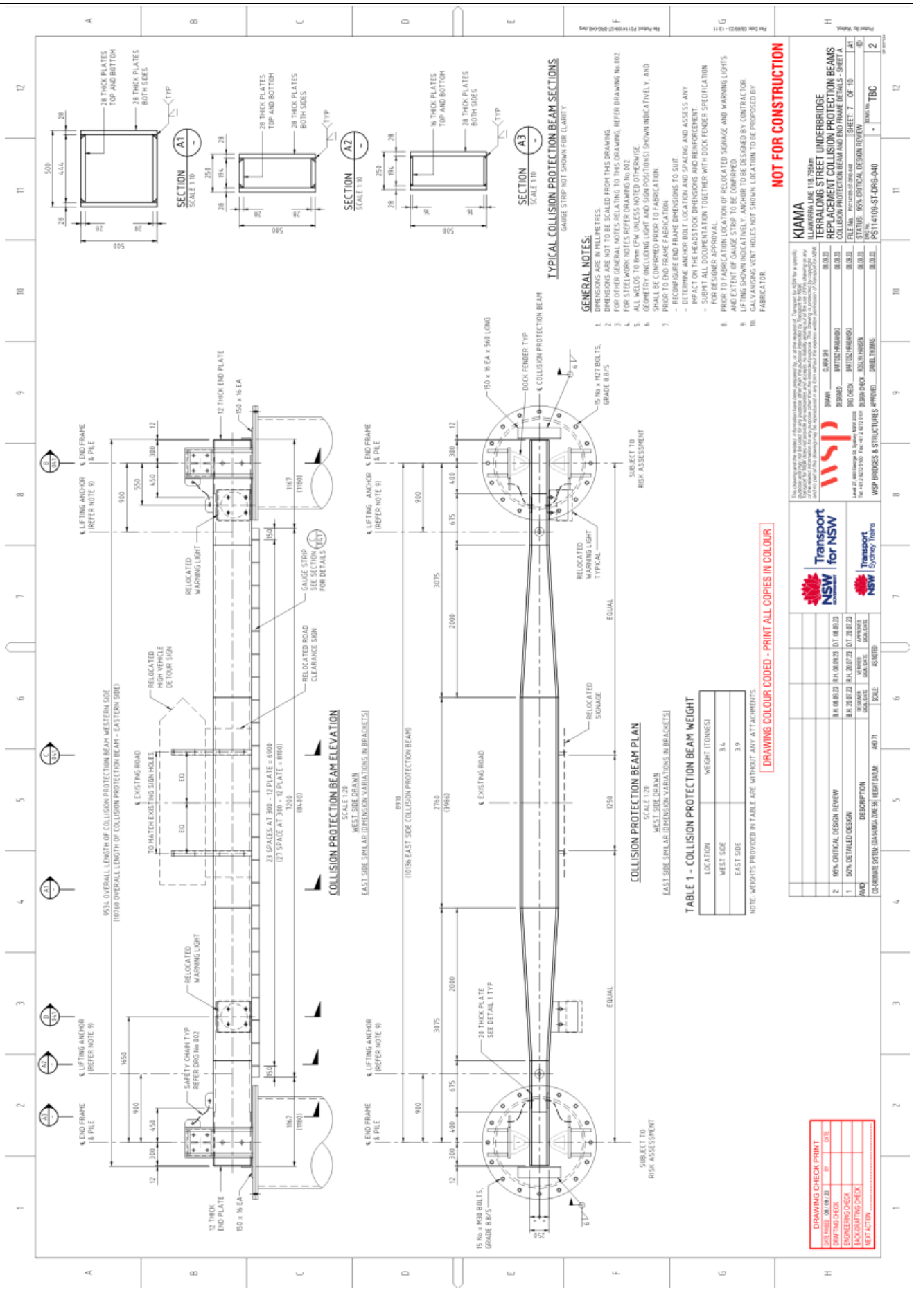
NO	REVISION	DATE	DESCRIPTION	APPROVED
2	95% CRITICAL DESIGN REVIEW	20/07/23		
1	90% DETAILED DESIGN	01/08/23		

**ELEVATION**  
 SCALE 1:20  
 FOR VALUES OF RL. A, RL. B, 'L', REFER TABLE 1 ON DRAWING No. 620

**φ1200 OD PILE WITH COLUMN EXTENSION**

**DRAWING CHECK PRINT**

DATE ISSUED: 20/07/23	BY: [Signature]
ENGINEERING CHECK: [Signature]	DATE: [Date]
PROJECT MANAGER: [Signature]	
NEXT ACTION:	



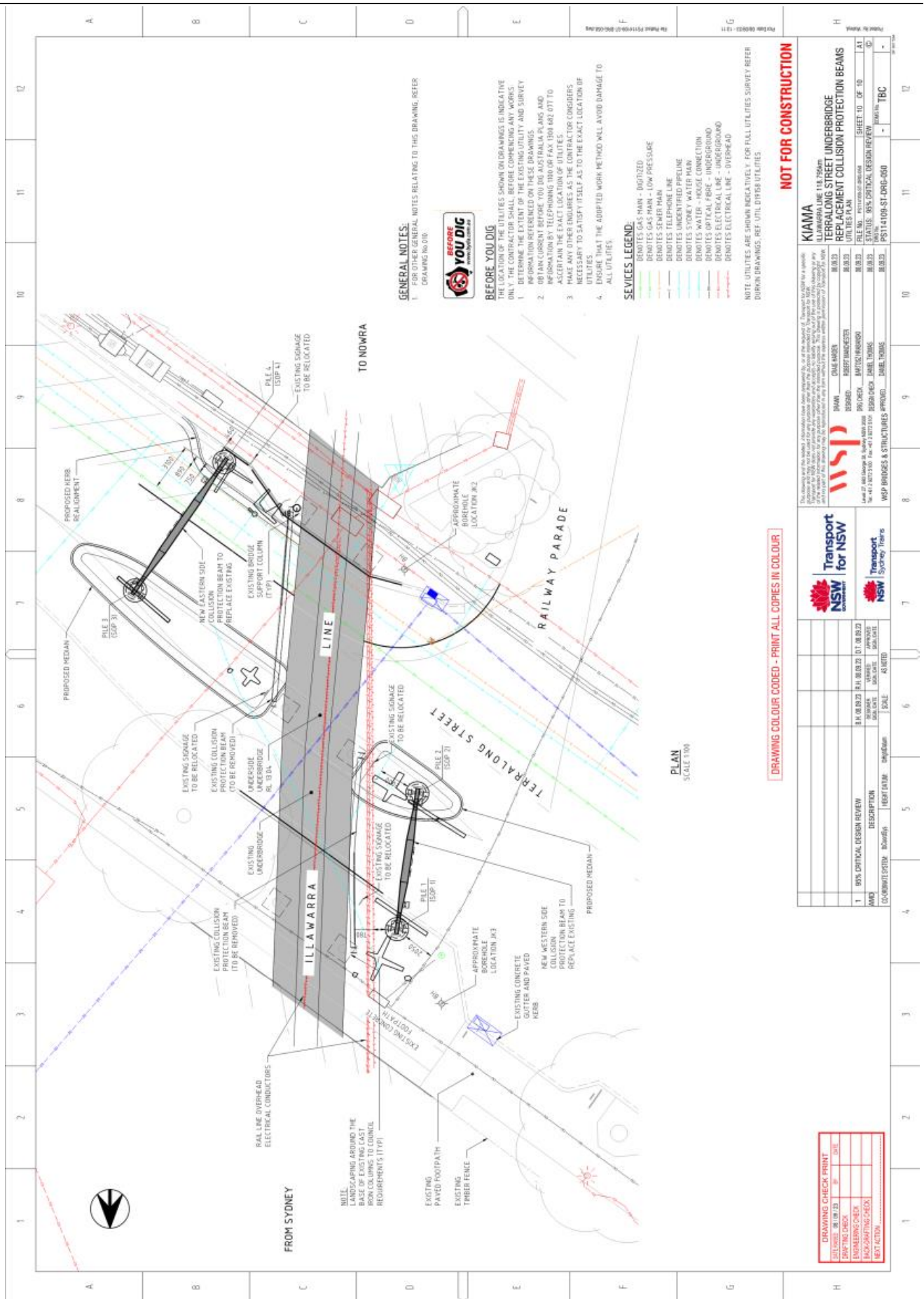






**Item 5.5 - Terralong Street Underbridge - Replacement Collision Protection Beams**

**Attachments 1 - TRANSPORT NSW - SYDNEY TRAINS - Terralong Street Underbridge - Replacement Collision Protection Beams - Drawings - PS114109-ST-DRG-COMBINED**



**GENERAL NOTES:**  
 1. FOR OTHER GENERAL NOTES RELATING TO THIS DRAWING, REFER DRAWING No. 010.



**BEFORE YOU DIG**  
 THE LOCATION OF THE UTILITIES SHOWN ON DRAWINGS IS INDICATIVE ONLY. THE CONTRACTOR SHALL, BEFORE COMMENCING ANY WORKS, DETERMINE THE EXTENT OF THE EXISTING UTILITY AND SURVEY INFORMATION REFERRED ON THESE DRAWINGS. THE CONTRACTOR SHALL OBTAIN INFORMATION BY TELEPHONING 1800 067 FAX 1800 683 077 TO ASCERTAIN THE EXACT LOCATION OF UTILITIES.  
 3. MAKE ANY OTHER ENQUIRIES AS THE CONTRACTOR CONSIDERS NECESSARY TO SATISFY ITSELF AS TO THE EXACT LOCATION OF UTILITIES.  
 4. ENSURE THAT THE ADOPTED WORK METHOD WILL AVOID DAMAGE TO ALL UTILITIES.

- SERVICES LEGEND:**
- DEMOTES GAS MAIN - DUCTED
  - DEMOTES GAS MAIN - LOW PRESSURE
  - DEMOTES SEWER MAIN
  - DEMOTES TELEPHONE LINE
  - DEMOTES UNIDENTIFIED PIPELINE
  - DEMOTES STONEY WATER MAIN
  - DEMOTES WATER - HOUSE CONNECTION
  - DEMOTES OPTICAL FIBRE - UNDERGROUND
  - DEMOTES OPTICAL FIBRE - OVERHEAD
  - DEMOTES ELECTRICAL LINE - OVERHEAD

NOTE: UTILITIES ARE SHOWN INDICATIVELY. FOR FULL UTILITIES SURVEY REFER DURKAN DRAWINGS, REF. UTIL D11568 UTILITIES.

**NOT FOR CONSTRUCTION**

**DRAWING COLOUR CODED - PRINT ALL COPIES IN COLOUR**

PLAN  
SCALE 1:100

<p><b>PROJECT INFORMATION</b></p> <p>PROJECT: TERRALONG STREET UNDERBRIDGE REPLACEMENT COLLISION PROTECTION BEAMS</p> <p>DATE: 08/09/22</p> <p>SCALE: AS SHOWN</p>																	
<p><b>REVISIONS</b></p> <table border="1"> <thead> <tr> <th>NO.</th> <th>DATE</th> <th>DESCRIPTION</th> <th>BY</th> <th>CHKD.</th> </tr> </thead> <tbody> <tr> <td>1</td> <td>08/09/22</td> <td>10% CRITICAL DESIGN REVIEW</td> <td>AMC</td> <td>AS/STED</td> </tr> </tbody> </table>						NO.	DATE	DESCRIPTION	BY	CHKD.	1	08/09/22	10% CRITICAL DESIGN REVIEW	AMC	AS/STED		
NO.	DATE	DESCRIPTION	BY	CHKD.													
1	08/09/22	10% CRITICAL DESIGN REVIEW	AMC	AS/STED													
<p><b>APPROVALS</b></p> <table border="1"> <thead> <tr> <th>ROLE</th> <th>NAME</th> <th>DATE</th> </tr> </thead> <tbody> <tr> <td>DESIGNER</td> <td>AMC</td> <td>08/09/22</td> </tr> <tr> <td>CHECKED</td> <td>AMC</td> <td>08/09/22</td> </tr> <tr> <td>APPROVED</td> <td>AMC</td> <td>08/09/22</td> </tr> </tbody> </table>						ROLE	NAME	DATE	DESIGNER	AMC	08/09/22	CHECKED	AMC	08/09/22	APPROVED	AMC	08/09/22
ROLE	NAME	DATE															
DESIGNER	AMC	08/09/22															
CHECKED	AMC	08/09/22															
APPROVED	AMC	08/09/22															
<p><b>CLIENT INFORMATION</b></p> <p>CLIENT: TRANSPORT NSW</p> <p>PROJECT NO: PS114109-ST-DRG-C00</p>																	
<p><b>PROJECT LOCATION</b></p> <p>PROJECT LOCATION: TERRALONG STREET UNDERBRIDGE REPLACEMENT COLLISION PROTECTION BEAMS</p>																	
<p><b>PROJECT STATUS</b></p> <p>PROJECT STATUS: 10% CRITICAL DESIGN REVIEW</p>																	
<p><b>PROJECT CONTACT</b></p> <p>PROJECT CONTACT: WSP PROJECTS &amp; STRUCTURES APPROVAL: JMBE.TOMBS</p>																	
<p><b>PROJECT DRAWING</b></p> <p>PROJECT DRAWING: PS114109-ST-DRG-C00</p>																	
<p><b>PROJECT SHEET</b></p> <p>PROJECT SHEET: SHEET 10 OF 10</p>																	
<p><b>PROJECT DRAWING</b></p> <p>PROJECT DRAWING: PS114109-ST-DRG-COMBINED</p>																	

## 5.6 Proposed Road Closure - South Coast Tour

CSP Objective: Outcome 4.3: Our built environment is safe and inclusive; our infrastructure is well planned and well managed.

CSP Strategy: 4.3.1 Create a regional network of interconnected roads, shared pathways and cycleways to maximise access to key destinations and facilities.

Delivery Program: 4.3.1.1 Manage recreation and open space infrastructure to cater for current and future generations

Item 5.6

### Summary

The event organiser Elite Energy is proposing to temporarily close a number of roads within the Foxground area to enable the proposed South Coast Cycling Tour, a cycling event planned to take place on Saturday, 7th September 2024. This tour is similar to the proposed Triathlon event planned for the October 2024, not yet supported by the LTC and requiring altered traffic arrangements.

The event organiser is proposing on closing the following roads on 7th of September 2024, between 8.00 a.m. and 2:00pm for the South Coast Tour:

- Donovan Rd
- Toolijooa Rd

The South Coast Tour cycling event with the event site being at Granties Maze, 25 Donovan Road, Foxground where parking is made available for event participants.

Recommended detour information has not been provided.

### Financial implication

Nil

### Risk implication

Traffic/parking congestion, pedestrian safety and access to properties inside and along the course. A Traffic Management Plan has been prepared for the event to address both vehicular and pedestrian access and safety.

### Policy

Section 138 Road Occupancy under the Roads Act.

### Consultation (internal)

Consultation with Manager Engineering & Technical Services and Manager Tourism & Events.

### Communication/Community engagement

Kiama Local Traffic Committee

### Attachments

- 1 TGS-SCT-2024 Traffic control management plan South Coast Tour

Report of RMS delegated formal items

5.6 Proposed Road Closure - South Coast Tour (cont)

---

2 South Coast Tour 2024 Proposed Road Closure

**Enclosures**

Nil

**RECOMMENDATION**

That the Committee provides initial advice on the proposed traffic management plans.

**Background**

The South Coast Cycling Tour is proposed to be held on Saturday 7<sup>th</sup> October 2024, the event is to be based at 25 Donovan Road, Foxground. The start and finish line will both be located on Donovan Rd, Foxground, utilising Granties Maze as the event site. Riders will complete a 28km cycle course, starting and finishing on Donovan Road. There will be a number of distances available throughout the day, utilising the same course – 28km, 50km, 72km, 94km.

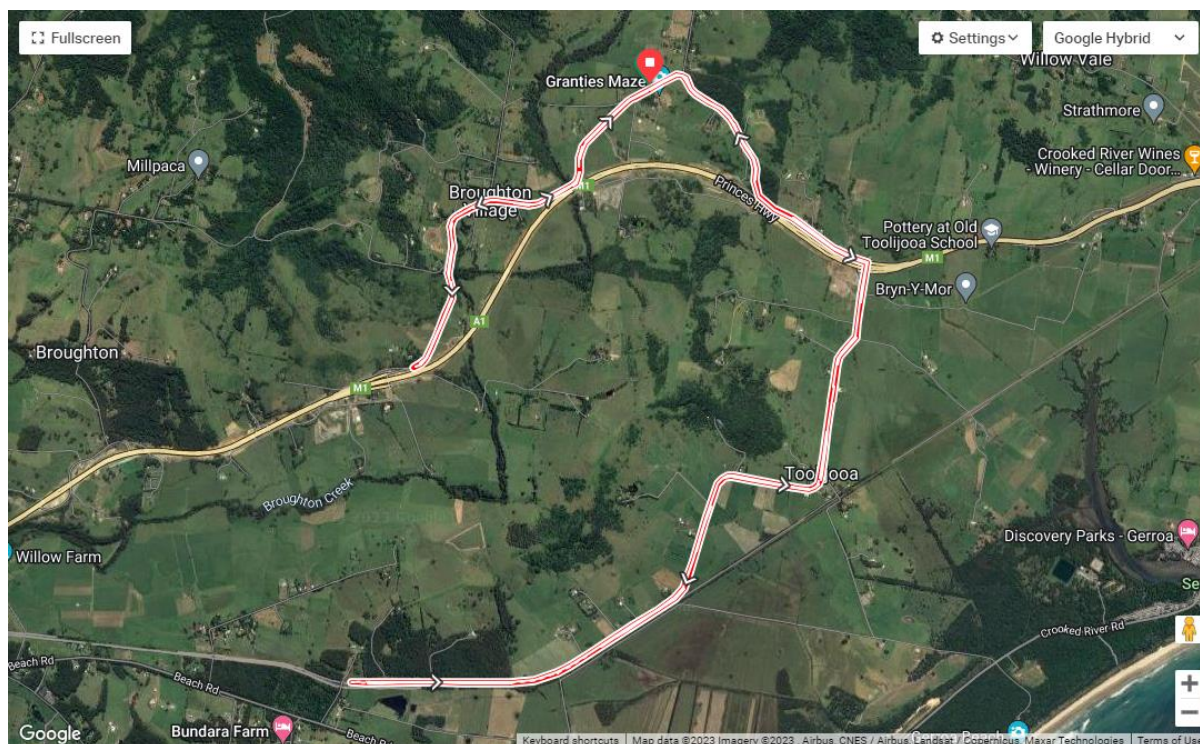
The event organiser is proposing on closing the following roads on 7<sup>th</sup> of September 2024, between 8.00 a.m. and 2:00pm.:

- Donovan Rd
- Toolijooa Rd

Access to approximately 100+ properties will be severely restricted during this period. Properties located near and north of Donovan Road including Foxground Road, and around Toolijooa Road. Access along Miller Lane and Sharpes Lane will also be restricted.

The on-road route of the proposed South Coast Cycling tours is shown below or can be found at the following link <https://ridewithgps.com/routes/44526889>.

5.6 Proposed Road Closure - South Coast Tour (cont)



Item 5.6

According to Elite Energy, traffic control measures included in this document are a result of consultation undertaken by Elite Energy, Kiama Municipal Council, Shoalhaven City Council, NSW Police & TfNSW (if required).

Concerns are regarding the access for emergency services to the properties affected by the road closure. The applicant is requested to liaise with the emergency services about the event. The applicant has assured Council in their previous application and on-site meeting for the Triathlon event that access for emergency services will be always available and a priority.

To ensure that local residents have ample opportunity to plan around the event it will be requested that local residents and business be notified by a letterbox drop 6 and 2 weeks prior to the event. It is also required that an advertisement be placed in a local newspaper notifying residents of the closure.

**Preliminary proposed endorsement**

*Supports that the traffic changes and road closures on 7th of September 2024, between 8.00 a.m. and 2:00pm for the South Coast Tour for the following roads:*

- *Donovan Rd*
- *Toolijooa Rd*

*The traffic changes and road closures with the event on 7th September 2024 from 8.00 a.m. to 2:00p.m.. be approved subject to organisers complying with the following conditions:*

## Report of RMS delegated formal items

## 5.6 Proposed Road Closure - South Coast Tour (cont)

1. *The event organiser complies with the Traffic Management Plans shown in Attachments.*
2. *The organisers shall be responsible for the supply, erection and removal of all traffic control devices, equipment and personnel associated with the implementation of the approved Traffic Management Plan.*
3. *Lodgment with Council of an application for a Section 138 Roads Act permit and payment of associated fee.*
4. *Notification of proposed traffic changes be given to police, local emergency services, businesses and residents effected by the closure.*
5. *An advertisement be placed in the local media advising of the closure and traffic changes at least 6 weeks and 2 weeks in advance of the event*
6. *Local residents and businesses located within the area of the road closure be notified of the proposed event 6 and 2 weeks prior to the event with a letter box drop, with a copy provided to Council*
7. *Access through the road closures for emergency services be available at all times.*
8. *Proof of public liability insurance complying with the Australian Prudential Regulatory Authority (APRA) being provided to Council prior to the event.*
9. *The organisers apply for and receive approval from Transport for NSW for any potential impacts on the State Road network (Road Occupancy Licence) and implementation of proposed changes to the M1 Princes Highway variable message signs.*

WWW.ELITEENERGY.CO.UK

# **SOUTH COAST TOUR**

# **Traffic Guidance Scheme**

# **Saturday 7th September 2024**

# **TGS-SCT-2024 (Plans 1 - 10)**

Copyright Elite Energy Pty Ltd 2014. This publication is copyright and remains the intellectual property of Elite Energy Pty Ltd. No part of it may be reproduced by any means without the prior written permission of Elite Energy Pty Ltd.









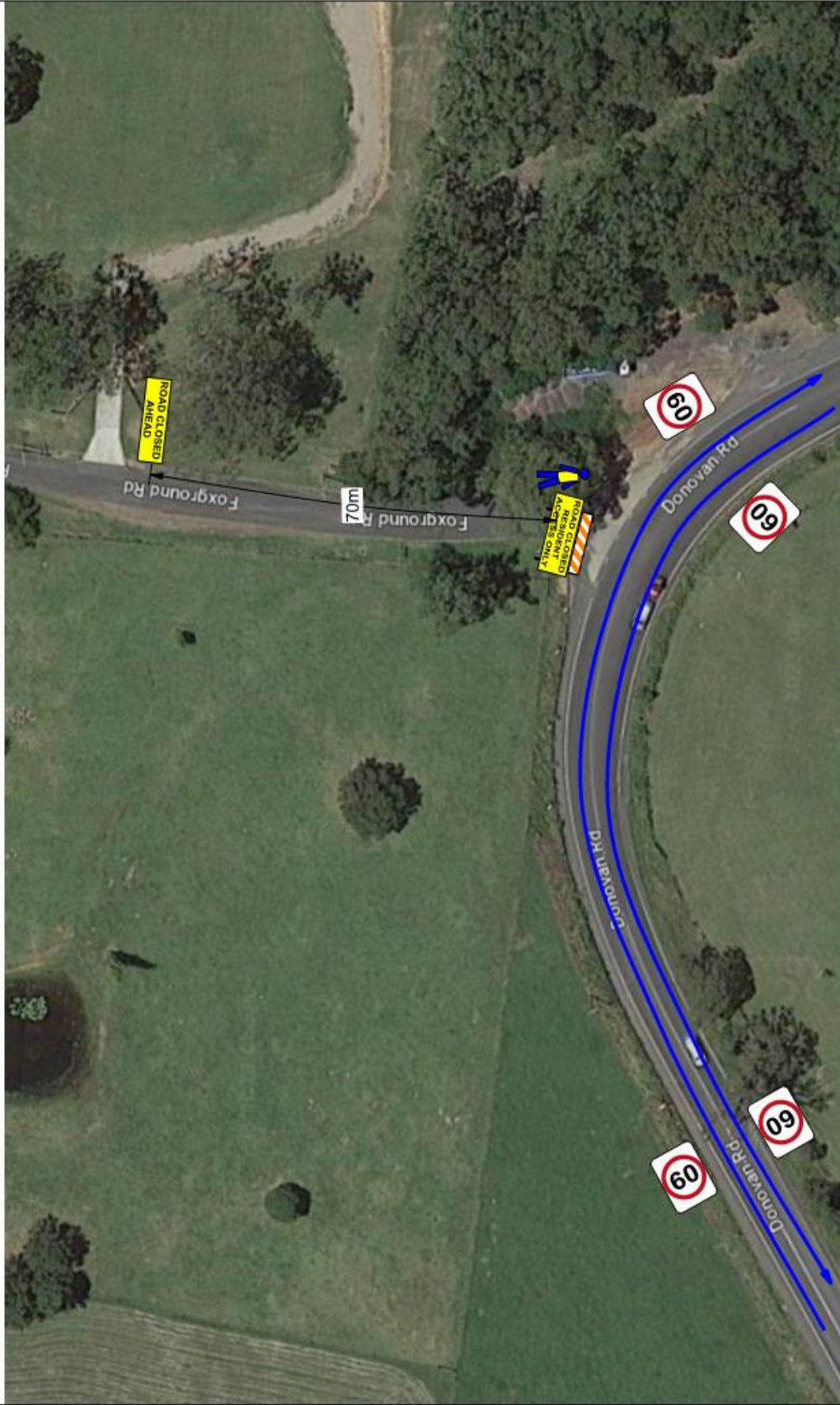








WWW.EIITS.COM



<p><b>elitsenergy</b> MAKING EVENTS MEMORABLE</p>	<p><b>LEGEND</b></p> <ul style="list-style-type: none"> <li>Accidental Traffic Control</li> <li>Approved Signage</li> <li>Barriers</li> <li>Cones</li> <li>Head/Crowd Bars</li> <li>Vehicle Movement</li> <li>Personnel</li> <li>Signage</li> <li>Sumo Traffic</li> <li>Cyclist Movement</li> <li>Bus Movement</li> </ul>	<p><b>NSW</b>   <b>SAFETYWORK NSW</b></p> <p><b>WORK, HEALTH &amp; SAFETY</b> <b>TRAFFIC CONTROL WORK</b></p> <p><b>Zeah</b> <b>McMILLAN</b> SCT No: 18221 15/11/2021 APP: TCM - PMZ</p>	<p><b>NEW SOUTH WALES</b></p> <p>DATE: 7 SEP 2024 AUTHOR: Zeah McMillan TMP No: South Coast Tour ROAD TYPE: 2W, 2L ROAD SPEED: 80km/hr TGS No: TGS-SCT-2024 Plan 6</p>	<p><b>Comments:</b></p>











# **SOUTH COAST TOUR**

## **2024**

### **SATURDAY 7TH SEPTEMBER 2024**

# **TRAFFIC MANAGEMENT PLAN**

## TABLE OF CONTENTS

PART 1 Event Overview

PART 2 Traffic Arrangements

PART 3 Maps

PART 4 VMS Strategy

PART 5 Operational Manual

PART 6 Contact List

PART 7 Traffic Guidance Scheme

PART 8 Risk Assessment

Item 5.6

Attachment 2

## PART 1 Event Overview

### TMP Event Overview

<b>Event Name:</b>	South Coast Tour
<b>Event Date:</b>	7 <sup>th</sup> September 2024
<b>Time:</b>	8:00am to 2:00pm
<b>Location:</b>	Granties Maze, 25 Donovan Rd, Foxground
<b>Event Organiser:</b>	Elite Energy
<b>TMP Version:</b>	Version 1
<b>Revision Date:</b>	26/09/2023
<b>Document Author:</b>	Zeah McMillan

#### This Traffic Management Plan reviewed by:

<b>Kiama Municipal Council:</b>	/ /	
<b>Shoalhaven City Council:</b>	/ /	
<b>NSW Police:</b>	/ /	
<b>Transport for NSW (if required – see below):</b>	/ /	

#### Authority of the TMP

This Traffic Management Plan (TMP) when reviewed by the above relevant authorities becomes the prime document detailing the traffic and transport arrangements under which the Kiama Triathlon will proceed.

Changes to the TMP require the approval of the above relevant authorities. All functional or single agency supporting plans (if any) are to recognise the primacy of this TMP and nothing contained in those plans may contravene any aspect of the TMP.

In the case of emergencies, or for the management of incidents, the NSW Police are not subject to the conditions of the TMP but will make every effort to inform the other stake holders of the nature of the incident and the Police response.

Transport for NSW (TfNSW) approval is required if the proposed event is to be held on or will impact upon a State Highway or Classified Road (see list below) or is within 100 metres of a Classified Road or any traffic signals. The applicant must also obtain a Road Occupancy Licence (ROL) from Transport for NSW (TfNSW) and must comply with all conditions of the ROL.

#### State Highways / Classified Roads

- State Highway No. 1 – Princes Highway – Full Length (State Highway)
- Main Road 261 (MR261) – Nowra / Moss Vale Road – Full Length (Main Road)

<b>Contents</b>	
<b>TMP Event Overview</b>	3
<b>Contents</b>	4
<b>Planning Contacts</b>	5
<b>Mission</b>	6
<b>Situation Analysis</b>	6
<b>Risk Assessment</b>	7
<b>Execution</b>	7
<b>General Outline</b>	7
<b>Physical Survey of Route</b>	9
<b>Survey of Route</b>	9
<b>Road Closures</b>	10
<b>Contingency Plans</b>	10
<b>Barricade Locations and Road Closure Times</b>	12
<b>Media</b>	12
<b>On-road Event Infrastructure</b>	13
<b>Ambulances</b>	13
<b>Medical</b>	13
<b>Port-A-Loos</b>	13
<b>Cleaning/removal/disposal</b>	13
<b>Public Transport Management</b>	13
<b>Volunteers and Event Marshals</b>	13
<b>Event Signage</b>	13
<b>Variable Message Signs</b>	13
<b>Access – Local Residents, Business, Emergency vehicles</b>	13
<b>Parking</b>	14
<b>Other Considerations</b>	14
<b>Attachments</b>	14

Item 5.6

Attachment 2



**Planning Contacts**

<b>Organiser</b>		<b>Position:</b>	Operations Manager	
<b>Phone:</b>	02 4423 7775	<b>Fax:</b>		<b>Mobile:</b>
<b>Address:</b>	15 Haigh Ave Nowra NSW 2541			
<b>E-mail:</b>				

<b>Kiama Municipal Council</b>	Helen Demertzis	<b>Position:</b>	Event Coordinator	
<b>Phone:</b>	02 4203 4900	<b>Fax:</b>		<b>Mobile:</b> 0484 546 123
<b>Address:</b>	11 Manning St Kiama NSW 2533			
<b>E-mail:</b>	<a href="mailto:events@kiama.nsw.gov.au">events@kiama.nsw.gov.au</a>			

<b>Kiama Municipal Council</b>	Nathan Zaremski	<b>Position:</b>	Civil Engineer	
<b>Phone:</b>	02 4232 0474	<b>Fax:</b>		<b>Mobile:</b>
<b>Address:</b>	11 Manning St Kiama NSW 2533			
<b>E-mail:</b>	<a href="mailto:nathan.zaremski@kiama.nsw.gov.au">nathan.zaremski@kiama.nsw.gov.au</a>			

<b>Shoalhaven City Council</b>	Renee Wenban	<b>Position:</b>	Event Liaison Officer	
<b>Phone:</b>	02 4429 3541	<b>Fax:</b>		<b>Mobile:</b> 0438 909 451
<b>Address:</b>	Bridge Road, Nowra NSW 2541			
<b>E-mail:</b>	<a href="mailto:Renee.Wenban@shoalhaven.nsw.gov.au">Renee.Wenban@shoalhaven.nsw.gov.au</a>			

<b>Shoalhaven City Council</b>	Jonathan Ash	<b>Position:</b>	Civil Engineer	
<b>Phone:</b>	1300 293 111	<b>Fax:</b>		<b>Mobile:</b>
<b>Address:</b>	Bridge Road, Nowra NSW 2541			
<b>E-mail:</b>	<a href="mailto:Jonathan.Ash@shoalhaven.nsw.gov.au">Jonathan.Ash@shoalhaven.nsw.gov.au</a>			

<b>NSW Police</b>	South Coast Police District	<b>Position:</b>		
<b>Phone:</b>	02 4421 9699	<b>Fax:</b>		<b>Mobile:</b>
<b>Address:</b>	88 Plunkett Street, Nowra 2541			
<b>E-mail:</b>				

<b>Transport for NSW</b>	Mitchell Vidler	<b>Position:</b>	Customer Operations Planner	
<b>Phone:</b>		<b>Fax:</b>		<b>Mobile:</b> 0491 215 301
<b>Address:</b>	Level 3, 193 Morgan St, Wagga Wagga NSW 2650			
<b>E-mail:</b>	<a href="mailto:mitchell.vidler@transport.nsw.gov.au">mitchell.vidler@transport.nsw.gov.au</a>			

### **Mission**

This document – Traffic Management Plan (TMP) aims to provide a plan for vehicular traffic and pedestrian control measures to assist in delivering safe logistical support of South Coast Tour, a cycling event planned to take place at Foxground on Saturday, 7<sup>th</sup> September 2024. The underlining objective is to minimise safety conflicts between vehicles and pedestrians and disruption to normal vehicular and pedestrian traffic.

The objectives of this TMP are:

- To provide a high-level description of the various traffic management elements needed to make the event a success
- Ensure effective separation between event patrons and participants from vehicular traffic.
- To provide a framework for each agency to use to develop their own lower-level plans.
- To serve as the key document that is agreed to by all parties and is the final approval to conduct the event.
- Minimise impact on non-event community and emergency services.

### **Situation Analysis**

On Saturday 7<sup>th</sup> September 2024, we are holding the South Coast Tour cycling event with the event site being at Granties Maze, 25 Donovan Road, Foxground, that will require altered traffic arrangements.

The event will be conducted by Elite Energy Pty Ltd and approved, sanctioned & insured by the national governing body, AusCycling.

Traffic Control measures included in this document are a result of consultation undertaken by Elite Energy, Kiama Municipal Council, Shoalhaven City Council, NSW Police & TfNSW (if required).

### Course Description

The start and finish line will both be located on Donovan Rd, Foxground, utilising Granties Maze as the event site. Riders will complete a 28km cycle course, starting and finishing on Donovan Road. There will be a number of distances available throughout the day, utilising the same course – 28km, 50km, 72km, 94km.

## TRAFFIC ARRANGEMENTS

Traffic control measures include simple hard and soft road closures, use of traffic control devices (traffic signs) to warn motorists of the changes in traffic conditions. It is noted that per NSW Workplace Health & Safety (WHS) law the Event Organiser – Elite Energy, bear individual and shared risks related to the safety of event patrons and motorists. Elite Energy is identified as being responsible for event related safety risks which are associated with non-transferable WHS statute-posed obligations and Common Law Duty of Care provisions. In this context, Elite Energy should continue to actively consult and take all reasonable measures to practically exercise their duty of care obligations.

### Risk Assessment

Part 8 of this document includes the completed risk assessment for the event.

### Execution

#### General Outline

The event requires highly coordinated efforts from a number of agencies:

#### Elite Energy

- Co-ordinates the logistics for holding the event, Venue Management / Event Production / Stage Management / Marshalling / Programming
- Arranges advertising for road closure locations, times, other traffic disruptions
- Prepares the Traffic Management Plan
- Provides traffic information signposting as identified in the TMP and associated Traffic Guidance Scheme (TGS).
- Provides resources and traffic management infrastructure for traffic control and road closures as identified in the TMP.
- Monitors traffic on all roads approaching James Oates Reserve to minimise traffic congestion on the day.

#### NSW Police

- May provide resources in the case of emergencies, or for the management of incidents.

**Physical Survey of Route**

**Survey of Route**

Item	Verified <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	Action Taken
All one-way streets are as described	<input type="checkbox"/> Yes <input type="checkbox"/> No	N/A
Block access to Church on Sunday	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	
Block access to local businesses	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	
Block Ambulance access	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	
Block fire station access	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	
Block heavy vehicle access	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	
Block hospital access	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	
Block local resident access	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	Resident access will be maintained when the roads are free of cyclists
Block Police vehicle access	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	
Block Public facility (football oval, carpark etc)	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	
Block public transport access	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	
Can route use alternatives such as bicycle tracks, paths, parks, bush tracks etc?	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	Road Cycling must be done on the road
Construction – existing, proposed that may conflict	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	
Distance measured is correct	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	
Lane widths	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	
Local access	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	
Numbers of lanes are as described	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	
Restricted movements – banned turns, heavy/high vehicles	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	
Road signage – existing/temporary	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	
Route impeded by traffic calming devices?	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	
Signalised intersections (flashing yellow? Point duty?)	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	
Tidal flows	<input type="checkbox"/> Yes <input type="checkbox"/> No	Not Applicable to traffic
Traffic generators – shopping centres, schools, churches, industrial area, hospitals	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	

Item	Verified <input checked="" type="checkbox"/> <input type="checkbox"/>	Action Taken
Traffic signals are as described	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	
Turning lanes are as described	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	

**Road Closures**

Part 2 (Traffic Arrangements) of this document provides details of proposed road closures during the event.

Maps showing road closures are in Part 3 (Maps) of this document.

**Contingency Plans**

This section of the Traffic Management Plan describes the contingency plans for the event. The contingency plan checklist identifies all possible issues/risks that may interfere with the event and the action to be taken to minimise the disturbance of the event.

**Contingency Plan checklist**

Issues/Risks	Applicable	Action Taken
Heavy Weather	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	See attached Contingency Plan
Lightening, hail, etc.	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	See attached Contingency Plan
Poor Lighting	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	See attached Contingency Plan
Flood hazard on the route	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	See attached Contingency Plan
Flood hazard at the parking area	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	See attached Contingency Plan
Parking during Wet weather	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	See attached Contingency Plan
Bush fire hazard	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	See attached Contingency Plan
Accident on the route	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	See attached Contingency Plan
Breakdown	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	See attached Contingency Plan
Absence of marshals and volunteers	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	See attached Contingency Plan
Absence of traffic signs crew	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	See attached Contingency Plan
Block public transport access	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	See attached Contingency Plan
Slow participants	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	See attached Contingency Plan
Delayed Event	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	See attached Contingency Plan
Cancellation of Event	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	See attached Contingency Plan
Security of participants	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	See attached Contingency Plan
Security of very important persons (VIP's)	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	See attached Contingency Plan
Dual purpose bridge	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	See attached Contingency Plan
COVID-19	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	See attached Contingency Plan

A Risk Management approach shall be an integral part of the planning for Kiama Triathlon. The risk, identification, assessment and control process is a legal obligation (as per the NSW Workplace Health and Safety Act and Regulation) and should be compliant with AS/NZS ISO 31000:2018. The event organiser is responsible for the management of risks arising from Kiama Triathlon.

It shall be noted that Traffic Management Plan (TMP) and particularly Traffic Guidance Scheme (TGS) are seen as risk control measures, but alone they cannot substitute and AS/NZS ISO 31000:2018 – compliant and detailed event Risk Assessment.

**Barricade Locations and Road Closure Times**

See Separate Traffic Guidance Scheme and Special Event Traffic Arrangements in Part 2 Traffic Arrangements

**Traffic Control**

- All traffic control including the placement and removal of barricades and/or regulation of traffic is to be carried out in accordance with the requirements of the approved Traffic Guidance Scheme and is to be undertaken by either Police or Traffic Controllers appropriately trained in accordance with the requirements of AS1742.3 and giving consideration to the TfNSW's Traffic Control at Worksites Manual.
- Consistent with the requirements of the WH&S regulations; in determining the appropriate resourcing for the event including placement of traffic controllers in accordance with the approved Traffic Guidance Scheme, the event controller (or traffic control supervisor) must consider a traffic controllers age, experience and level of competence to complete any tasks assigned as well as ensure the extent of information, instruction and training provided to all traffic control resources is appropriate to complete their tasks safely.

**Media**

There will be publicity about the event with our associated supporters and partners.

**On-road Event Infrastructure**

**Ambulances**

Emergency access lanes have been provided to allow emergency access to road closures areas.

**Medical**

There are medical personnel onsite and mobile for the duration of the event.

**Port-A-Loos**

There will be port-a-loos located at the event site.

**Cleaning/removal/disposal**

There will be wheelie bins and skips provided. During the event there will be staff servicing the area.

**Barricades**

Road closure barriers will be installed by Accredited Traffic Controllers.

**Public Transport Management**

Public transport companies will be notified of our event and will adjust/continue to use their normal route under the instructions from traffic controllers.

**Volunteers and Event Marshals**

Volunteers and event marshals are given verbal induction on arrival, provided with Hi-Vis vests and contacts of event and medical staff. Volunteers and event marshals are placed on the course by the staff member in charge with specific instructions on what their role involves in their position. Communications between volunteers and staff will be done throughout the day via mobile phone for breaks and any other issues.

**Event Signage**

Advanced notification and event signage per legislative requirements will be placed at key entry points to the affected area. A letterbox notification to all affected residents and businesses will be carried out.

**Variable Message Signs**

Part 4 of this document details the VMS Strategy that will be deployed to inform motorists of changed traffic conditions.

**Access – Local Residents, Businesses, Emergency vehicles**

**Local Resident and Business Access**

Residents and businesses in and around Broughton, Broughton Village, Foxground and Toolijooa will receive a letterbox drop notice with a minimum 21 days' notice, with



road closures and alternate routes outlined. All access for residents will be maintained throughout the day when it is safe to do so. All traffic will be advised to giveaway to cyclists and follow reduced speed limits.

#### **Emergency Vehicle Access**

The road closures allow for emergency vehicle access. NSW Ambulance, Police and Fire Brigade will be informed of changed conditions by Elite Energy.

#### **Parking**

Dedicated on-site parking is available for this event.

#### **Other Considerations**

##### **Television**

Television crew may be present on the day but will have zero impacts on roads as they will be located at the event site. Further information will be provided in the event that a television crew attends the event.

##### **Major Hotels and Other Businesses**

All hotels, businesses and caravan parks will be informed of our event with no impact on these.

#### **Attachments**

- COC
- TGS
- SWMS
- CONTINGENCY PLAN
- APPLICATION FOR ROAD & FOOTPATH OCCUPATION PERMIT

## PART 2 Traffic Arrangements

### **SPECIAL EVENT TRAFFIC ARRANGEMENTS**

#### **Road Closures**

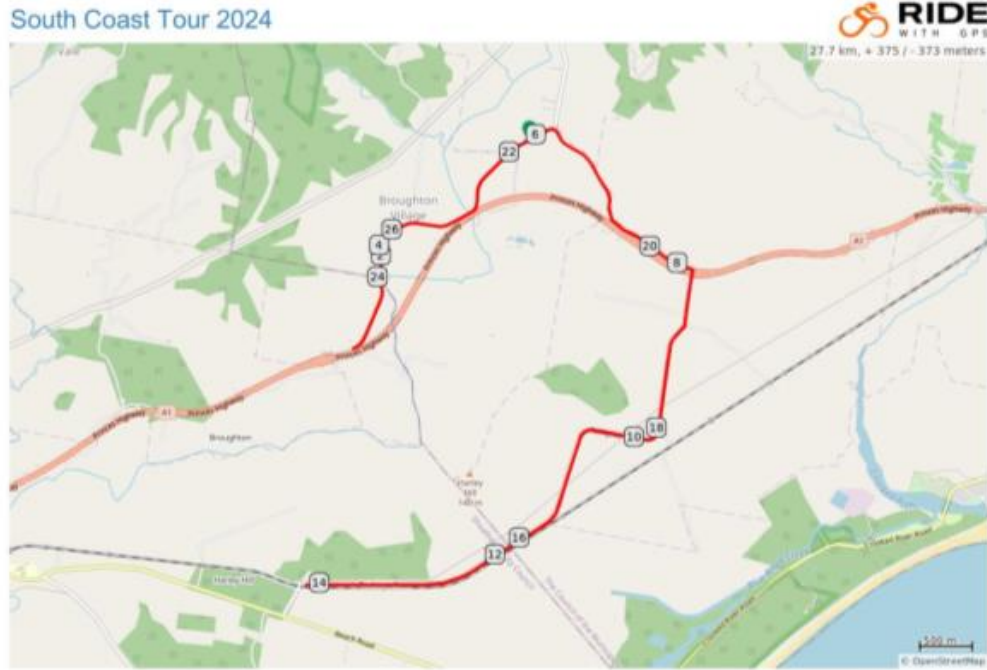
For the safety of competitors, spectators and the general public the following road closures will be required:

The following road closures (as per the attached TGS-SCT-SAT 2024) are required from approximately 8am to 2pm Saturday 7<sup>th</sup> September 2024:

1. Donovan Rd
2. Toolijooa Rd

### PART 3 Maps

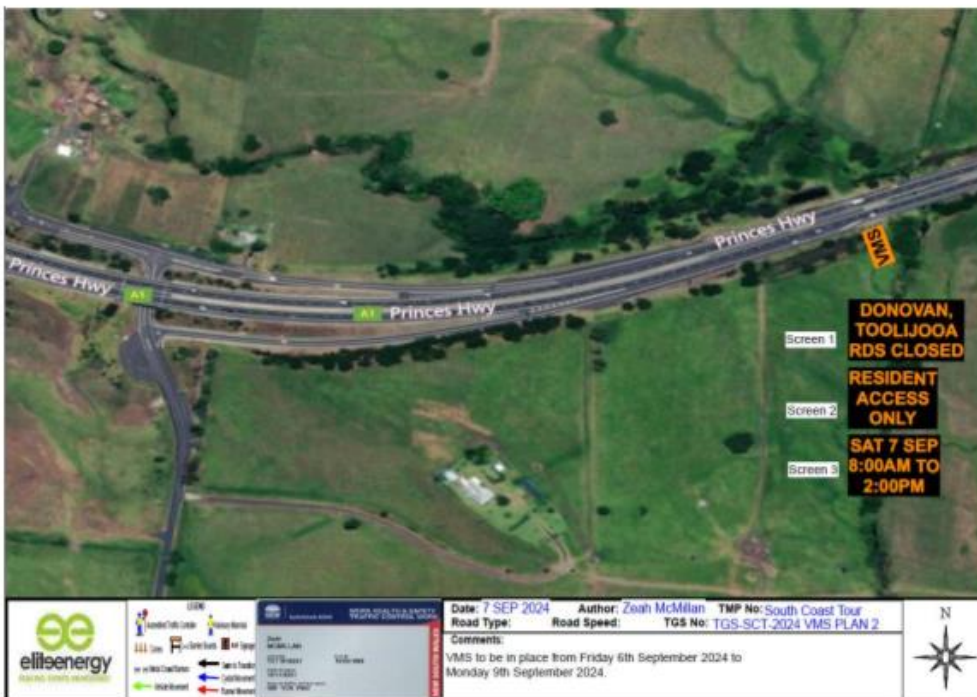
#### Course Map



Item 5.6

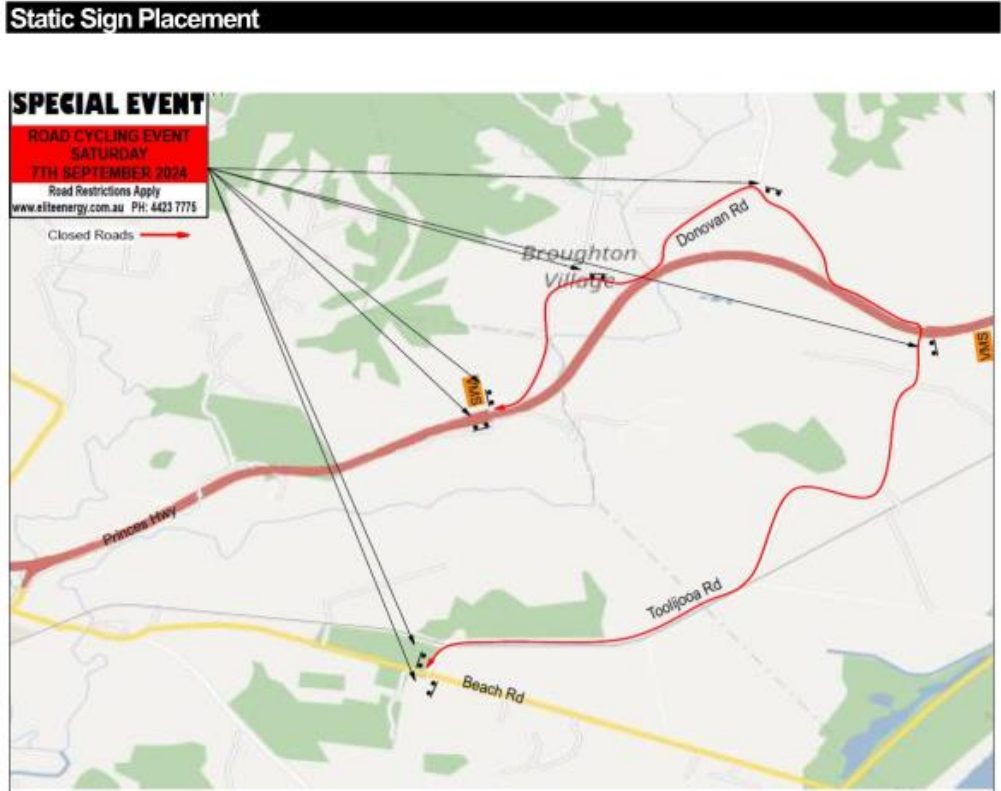
Attachment 2

PART 4 VMS Strategy



Item 5.6

Attachment 2



Item 5.6

Attachment 2

## PART 5 Operational Manual

### Event Schedule

To be confirmed closer to event date.

*Subject to change*

Item 5.6

Attachment 2

## PART 6 Contact List

**KIAMA TRIATHLON**

<b>NAME</b>	<b>ACTING FOR</b>	<b>TELEPHONE</b>	<b>MOBILE</b>	<b>EMAIL</b>
Kyle Anderson	Operations Manager, Elite Energy	02 4423 7775	0466 302 658	<a href="mailto:kyle@eliteenergy.com.au">kyle@eliteenergy.com.au</a>
Zeah McMillan	Traffic Manager, Elite Energy	02 4423 7775	0412 722 209	<a href="mailto:zeah@eliteenergy.com.au">zeah@eliteenergy.com.au</a>
Mark Emerton	Race Director, Elite Energy	02 4423 7775	0413 765 248	<a href="mailto:emo@eliteenergy.com.au">emo@eliteenergy.com.au</a>
Irene O'Brien	Volunteer Manager Elite Energy	02 4423 7775	0407 221 510	<a href="mailto:volunteer@eliteenergy.com.au">volunteer@eliteenergy.com.au</a>
Christie McIlvrde	Media Liaison	02 4423 7775	0425 242 405	<a href="mailto:christie@eliteenergy.com.au">christie@eliteenergy.com.au</a>
Insurance	AusCycling	1300 137 397		

PART 7 Traffic Guidance Scheme

See attached TGS-SCT-2024

Item 5.6

Attachment 2



PART 8 Risk Assessment

See attached SWMS

Item 5.6

Attachment 2

**4 GENERAL BUSINESS**

**5 CLOSURE**